



prepared by
KLM GERNER
CONSULTING GROUP



bridge street business precinct
urban design framework : **implementation** december 2003

TABLE OF CONTENTS

| | |
|---|----|
| 1.0 INTRODUCTION | 1 |
| 1.1 What is an Urban Design Framework? | |
| 1.2 Why does Bridge Street need a UDF? | |
| 1.3 The aim of the UDF | |
| 1.4 How was the UDF prepared? | |
| 1.5 How is the UDF structured? | |
| 1.6 The Study Area | |
| 2.0 THE VISION | 3 |
| 3.0 RECOMMENDATIONS | 4 |
| 3.1 Landscape Character and Public Realm Improvements | |
| 3.2 Traffic and Parking | |
| - 3.2.1 Car Parking Precinct Plan and 'Cash in Lieu' Payments | |
| 3.3 Signage | |
| 3.4 Land Use and Planning | |
| - 3.4.1 Zoning Recommendations | |
| - 3.4.2 Policy Recommendations | |
| - 3.4.3 Further Work Recommended | |
| 3.5 Bridge St Business Precinct Design and Development Guidelines | |
| 4.0 PRIORITIES AND COSTINGS | 16 |

LISTING OF MAPS

- A. Site Context
- B. Short Term Works : 1-5 Years
- C. Long Term Works : 5-10 Years
- D. Proposed Landscape Species
- E. Planting Zones within Precinct
- F. Proposed Works – Sherbourne Road
- G. Proposed Bridge Street Works – A
- H. Proposed Bridge Street Works –B
- I. Proposed Bridge Street Works – C
- J. Proposed Hard Landscape Works
- K. Proposed Wall Concept - 1
- L. Proposed Wall Concept - 2
- M. Proposed Signage Concept – 1
- N. Proposed Signage Concept – 2
- O. Suggested Activity Precincts





1.0 INTRODUCTION

1.1 WHAT IS AN URBAN DESIGN FRAMEWORK?

Urban Design Frameworks (UDF) are part of the Victorian State Government's *Pride of Place* program, which enables the State Government to work together with Councils, residents, traders and property owners to develop urban design solutions that help communities prosper.

UDFs are strategic planning tools that set out an integrated design vision for the desired future development of urban places by translating the broad aims of Councils *Municipal Strategic Statement (MSS)* into practical urban design actions at the local level. UDF recommendations are then translated into a series of actions that include public improvement works, design and development guidelines and the introduction of new land use zones.

The value of UDFs is increasingly recognised by Councils as an essential step towards achieving the best long-term outcomes that reflect community aspirations while providing a stimulus for private investment. In general, benefits include improved amenity, vitality and attractiveness along with measurable outcomes such as increased property values, greater visitation levels, new private investment, reduced crime and improved safety.

1.2 WHY DOES BRIDGE STREET NEED A UDF?

Traditionally one of Nillumbik's key industrial precincts, the Bridge Street Business Precinct has gone through some change in recent years as a result of the loss of some industrial uses together with growing pressure for service based uses including peripheral and bulky goods sales.

Located to the west of the Eltham township, Bridge Street provides an important gateway. However, unlike the northern and southern gateways into the Eltham township, the Bridge Street Business Precinct has suffered from a lack of specific planning controls, which has left the precinct without the benefit of a clear vision. This has resulted in a poor quality physical environment that suffers from a poor image, access difficulties and no real strategic direction

Following the strategic work undertaken for both the northern and southern gateways, Council recognises the opportunity to provides a new strategic direction for the Bridge Street Business Precinct by preparing a UDF that strengthens its weaknesses whilst building on the variety of opportunities that it currently offers.

1.3 THE AIM OF THE UDF

The aim of the UDF is to provide a strategic framework that amongst other things, provides a clear development framework for all future development within the Bridge Street Business Precinct whilst recommending a number of improvements to enhance the Precinct's image and identity.

The Project Brief identified the following aims and objectives:

- *To develop a sustainable urban design framework which guides the form, scale and nature of future development and/or redevelopment of privately owned sites in an innovative and creative manner.*
- *To develop a framework which will guide the form and character of works undertaken within public areas of Bridge Street, and other streets in the precinct, to enhance the image and identity of the area;*
- *To enhance the visual appearance and attractiveness of Bridge Street as it fulfils its important function as a Gateway to Eltham;*
- *To extend and enhance the gateway strategies previously undertaken by Council;*
- *To provide the opportunities for businesses in the Bridge Street precinct to contribute to the redevelopment of this strip retail/industrial area;*
- *To enhance the economic viability, accessibility and vibrancy of this locality;*
- *To contribute to the opportunities for Council and landowners to work in partnership to change the perception of Bridge Street, from being the back entrance to Eltham to being one of the premier gateways to the township.*

1.4 HOW WAS THE UDF PREPARED?

In July 2002, Nillumbik Shire Council engaged consultants to commence preparation of the Bridge Street Business Precinct UDF.

To assist in the preparation of the UDF, consultation was held with a variety of stakeholders that included representatives from the:

- Eltham Chamber of Commerce
- Eltham Gateway
- Landowners within the Study Area, and
- Business Operators within the Study Area.

The preparation of the UDF was managed by a Steering Committee comprising representatives of the Department of Infrastructure and Nillumbik Shire Council.

1.5 HOW IS THE UDF STRUCTURED?

The Urban Design Framework is presented in the form of two reports.

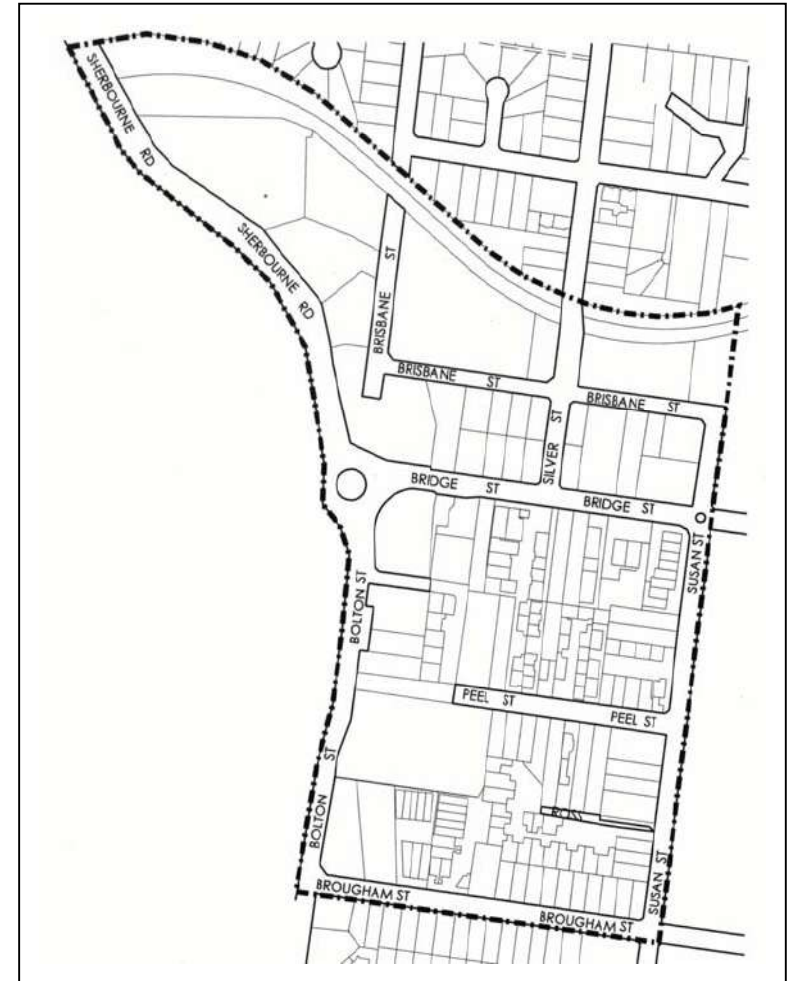
- An Implementation Report which outlines a series of actions relating to public improvement works, design and development guidelines and the introduction of new land use zones, and
- A Background Report which describes the existing conditions of the Bridge Street Business Precinct, outcome of the consultation process and the relevant sections of Council's MSS and planning scheme provisions.

1.6 THE STUDY AREA

The study area relates to the business and industrial precinct in Bridge St, Eltham. It includes those areas along Silver, Brisbane and Susan Streets (south to Brougham St) which abut the Bridge St area and form part of the business and light industrial precinct.

It is located south west of the Eltham Township on the opposite side of Diamond Creek.

As the main thoroughfare through the study area, Bridge St acts as a key gateway for the township of Eltham and the broader Nillumbik Shire. Bridge Street links Eltham with the Western Ring Road - Greensborough Bypass and the Greensborough Shopping Centre at Main Street Greensborough.



The Study Area



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Consulting
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2.0 THE VISION

In preparing the Bridge Street Business Precinct UDF, the following vision was adopted by the Steering Committee.

"The identity and functionality of the Bridge Street Business Precinct will be strengthened to create a viable, accessible and vibrant place in which to work, visit and travel through."

This vision was then translated into a series of more detailed goals and objectives that are outlined as follows:

- *To improve the functionality of the Bridge Street Business Precinct for the benefit of businesses, customers and the local community.*
- *To improve the appearance of Bridge Street as a gateway to Eltham.*
- *To improve the visual integration of the precinct with the surrounding environment.*

This will be achieved by,

- *Increasing employment opportunities by allowing office use in strategic areas;*
- *Better controlling the built form, landscaping and car parking requirements in all new development;*
- *Ensuring safe vehicle and pedestrian movement along Bridge St;*
- *Providing a continuous landscape treatment along Bridge St that links with surrounding ridges and parkland; and*
- *Improving integration with the Eltham Town Centre and the southern gateway.*



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3.0 RECOMMENDATIONS

The issues, constraints and opportunities identified during and following the existing conditions analysis, together with the feedback received during the consultation process and the project vision, form the basis to the recommendations for the Bridge Street Business Precinct. Specifically, these recommendations are translated into a series of actions that relate to public improvement works, design and development guidelines and the introduction of new land use zones. These actions are outlined in the following categories.

- 3.1 *Landscape Character and Public Realm Improvements*
- 3.2 *Traffic and Parking Proposals*
- 3.3 *Signage*
- 3.4 *Land Use and Planning Recommendations*
- 3.5 *Bridge Street Business Precinct Design and Development Guidelines*



Each action is then identified as work to be undertaken within one of the following areas along with the agency/persons responsible for implementation.

- *Management*
- *Capital Works, and*
- *Further Work Required*

It is important to note that while the land use, planning and signage recommendations apply to the whole precinct, the landscape and traffic recommendations have, where possible, been categorised by streets. Bridge Street is of particular focus due to its prominence as an entryway to Eltham. See *Maps B and C* for short and long term recommendations.



3.1 LANDSCAPE CHARACTER AND PUBLIC REALM IMPROVEMENTS

In order to address the lack of continuity and disorder within the study area, specific landscape works that include hard treatments, soft treatments and traffic and pedestrian controls have been proposed. (Maps B, C).

Planting within the business precinct will be in accordance with the *Proposed Landscape Species* list on Map D. This selection of street trees, small trees and shrubs, grasses and groundcovers has been selected from the *Nillumbik Shire Live Local Plant Local Guide*. Planting guidelines for specific zones is outlined in Map E.

Proposed works are recommended on a street by street basis, with greater emphasis given to Bridge Street due to its role as a major entrance way to Eltham and brings continuity and identity to this major road that also connects to the Business Precinct. By focusing particular physical works along Bridge Street, the business zone will begin to build an identity of its own. See Maps G to L for detailed diagrams and descriptions on the proposals for Bridge Street

At key points along Bridge Street, with emphasis on intersections, an insitu concrete feature wall is proposed to unify the street and define the street edge. These walls contribute to the identity of the street and offer options for branding the business precinct. See Maps J,K and L for more detail

Recommended traffic changes with regard to particular streets is also mentioned here.

See Maps B to P for detailed diagrams and descriptions



LANDSCAPE CHARACTER AND PUBLIC REALM IMPROVEMENTS (cont)

| SILVER STREET | | |
|---------------|---|-----------------------------------|
| | Task | Responsibility |
| Capital Works | <ul style="list-style-type: none"> Install kerb and channel Rationalise existing carparks with linemarking | Council Council/ landowners |
| Management | <ul style="list-style-type: none"> Encourage planting in private zones in accordance with planting guidelines (Maps D,E) | Council/ landowners |

| PEEL STREET | | |
|---------------|--|--------------------------------------|
| | Task | Responsibility |
| Capital Works | <ul style="list-style-type: none"> Install additional street lighting; Seal and linemark carparking spaces. Install feature corporate signage to advertise businesses not visible from major roads (Maps M,N) | Council Council Landowners |

| SHERBOURNE ROAD | | |
|-----------------|--|--|
| | Task | Responsibility |
| Capital Works | <ul style="list-style-type: none"> Provide pedestrian guardrail along existing footpath Remove weeds and revegetate creek line and embankment to strengthen gateway Construct a shared pathway for recreational cyclists along rail reserve as a Bridge Street alternative. Consider boardwalk style footpath down into site and tables for picnics as a long-term option. | Council Council/Melbourne Water, etc Council/VicTrack Council |
| Management | <ul style="list-style-type: none"> Investigate options for acquisition or leasing of Hat Factory land south of creek for public use in the context of securing the area due to its cultural significance. Investigate interest in developing a Friends Group to plant and maintain the site. | Council |

| BOLTON STREET | | |
|---------------|---|---------------------------------------|
| | Task | Responsibility |
| Capital Works | <ul style="list-style-type: none"> road widening, kerb and channel, footpath and provision for a guard rail | Council |
| Management | <ul style="list-style-type: none"> Upgrade Bolton St to Main Rd status Investigate the possibility of planting to the front and rear of developments on vacant land | Council Council/ landowners |

| OTHER | | |
|---------------|--|---------------------------------|
| | Task | Responsibility |
| Capital Works | <ul style="list-style-type: none"> Provide a recreational trail for pedestrians and cyclists along the southern side of the railway line for increased circulation and to establish links with Eltham Township. (Maps B,C) Linemark carparking spaces in the redundant section of Brisbane St, directly behind Red Rooster as an all day parking area for traders. | Council/VicTrack Council |
| Management | <ul style="list-style-type: none"> Encourage private landowners to plant in private garden beds to strengthen the visual effect of the street and create a sense of continuity. Plants and mulch for such a scheme could be supplied by council and be planted according to the planting guidelines put forth in Maps D and E. | Council/ landowners |
| Further Work | <ul style="list-style-type: none"> Investigate water sensitive drainage options such as swale drains in wider road reserves, particularly along the Susan Street parkland. | Council |

| BROUGHAM STREET | | |
|-----------------|---|----------------|
| | Task | Responsibility |
| Capital Works | <ul style="list-style-type: none"> Seal and linemark existing carpark spaces | Council |

| BRIDGE STREET | | |
|---------------|--|------------------------|
| | Task | |
| Capital Works | <ul style="list-style-type: none"> Replace damaged kerb and channel along the northern side | Council |
| | <ul style="list-style-type: none"> Install new 1800 wide concrete footpath curving between existing trees with tactile pavers and ramps at major crossing points. | Council |
| | <ul style="list-style-type: none"> Provide low concrete wall (500mm high) to unify street, act as a retaining wall, anchor signage and provide informal seating along the street (Maps J,K,L) | Council/ Landowners |
| | <ul style="list-style-type: none"> Remove existing volcanic rocks and other roadside hazards | Council |
| | <ul style="list-style-type: none"> Plant road reserves with native grasses (Maps D, E) in granitic sand. This will provide variation as well as reducing maintenance. | Council |
| | <ul style="list-style-type: none"> Continue planting on roundabouts. Roundabouts identified as potential location for future art installation. | Council/ Landowners |
| | <ul style="list-style-type: none"> Upgrade Street Lighting: Consider incorporating lighting and signage. | Council |
| Management | <ul style="list-style-type: none"> Negotiate with traders to provide a coordinated approach to signage (Maps M, N) | Council/traders |
| | <ul style="list-style-type: none"> Strengthen street landscaping by encouraging private landowners to plant gardens with Council provided plants and mulch according to schedule (Maps D&E) | Council/ Landowners |
| | <ul style="list-style-type: none"> Encourage contribution to wall construction during redevelopment of private sites | Council/ Landowners |
| Further Work | <ul style="list-style-type: none"> Prepare a submission for the provision of underground power along the length of Bridge Street as a focus: | Council |

3.2 TRAFFIC AND PARKING

Consideration needs to be afforded to existing and future land use developments and the possible impact upon traffic and parking within the precinct. Council and the traders within the precinct need to be prepared for future development pressures, which may arise within the precinct, and within the surrounding area.

There are several areas where significant on street parking exists which are often underutilised ie. the Parking in Brisbane street (north). Where possible employers should encourage employees to park in these locations, and walk to their place of employment. Naturally this will release valuable on-premises parking spaces for customers and delivery vehicles.

On larger sites consideration should be afforded to new developments providing underground car parking. Naturally the cost associated with providing this should be offset against any car parking contribution requirements.

3.2.1 Car Parking Precinct Plan and 'Cash in Lieu' Payments

As a means of dealing with ongoing car parking issues throughout the Bridge St precinct, it is recommended that a Parking Precinct Plan be prepared for the Bridge Street Business Precinct.

A car parking precinct plan is a strategic plan that may specify different requirements to those set out in the table at Clause 52.06-5. It is based on an assessment of car parking demand and supply within a defined area and recommends certain outcomes to be achieved.

Any location, financial, landscape or other actions or requirements necessary to implement the parking precinct plan need to be identified. The opportunity exists to implement a car parking strategy which allows for uses which require an intensification of car parking to contribute to improvements to the existing car parking spaces and provide additional car parking in the future.

Prior to the preparation and implementation of a car parking precinct plan, new developments are required to provide adequate car parking on site.

The variety of uses permissible in the proposed Business 3 zone (ie. restricted retail) will allow activities, which will require higher car-parking rates than compared to traditional Industrial uses (ie. Warehouse).

The introduction of the Business 3 Zone should occur concurrently with the incorporation of a car parking policy for this precinct.

In addition to the traffic-related improvements that are put forward in the previous section, the following actions are suggested in Bridge St.

| BRIDGE STREET TRAFFIC WORKS (MAP. G,H,I) | | |
|---|---|---|
| | Task | Responsibility |
| Capital Works | <ul style="list-style-type: none"> Remove redundant vehicle crossovers Provide on road bicycle lanes in Bridge St. | <p><i>Council</i></p> <p><i>Council/VicRoads</i></p> |
| Management | <ul style="list-style-type: none"> Review crossover widths as opportunities arise; Formalise parking layouts, line marking and crossovers to increase the number of parking spaces and reduce impact on adjoining road Retain central median solely for use of right turning traffic; Relocate the north bus stop to the east of the pedestrian crossing; Reinstate 'No parking' signage with provision for buses and postal vehicles. | <p><i>Council</i></p> <p><i>Council</i></p> <p><i>Council</i></p> <p><i>Council/bus company</i></p> <p><i>Council</i></p> |
| Further Investigation | <ul style="list-style-type: none"> Investigate the need for a signalised pedestrian crossing directly east of Silver Street. Investigate the opportunity to provide access to loading facilities from rear lanes or streets; Investigate the need for a pedestrian crossing at the roundabout on Bolton Street Review location of the Bus stop on the northern side of Bridge Street where the new development is proposed. | <p><i>Council/VicRoads</i></p> <p><i>Council/Landowners</i></p> |



- Apply planning controls to whole precinct:
- use - increase employment opportunities
 - height controls
 - front setbacks
 - landscaping requirements
 - car parking provision
 - limited colour controls
 - signage controls

Sherbourne Rd

Provide guard rail along footpath.

Remove weeds and revegetate creekline and batter in Brisbane St.

Bolton St

Council proposal to upgrade to Main Road status.

Investigate planting possibility to front and rear of new premises.

Peel St

Install additional lighting.

Brougham St

Seal and linemark carparks.

Bike paths

Utilise linemarking and bike tracks to establish cycling links beyond the precinct.

Silver St

Install kerb and channel.

Provide linemarking for carparks.

Susan St

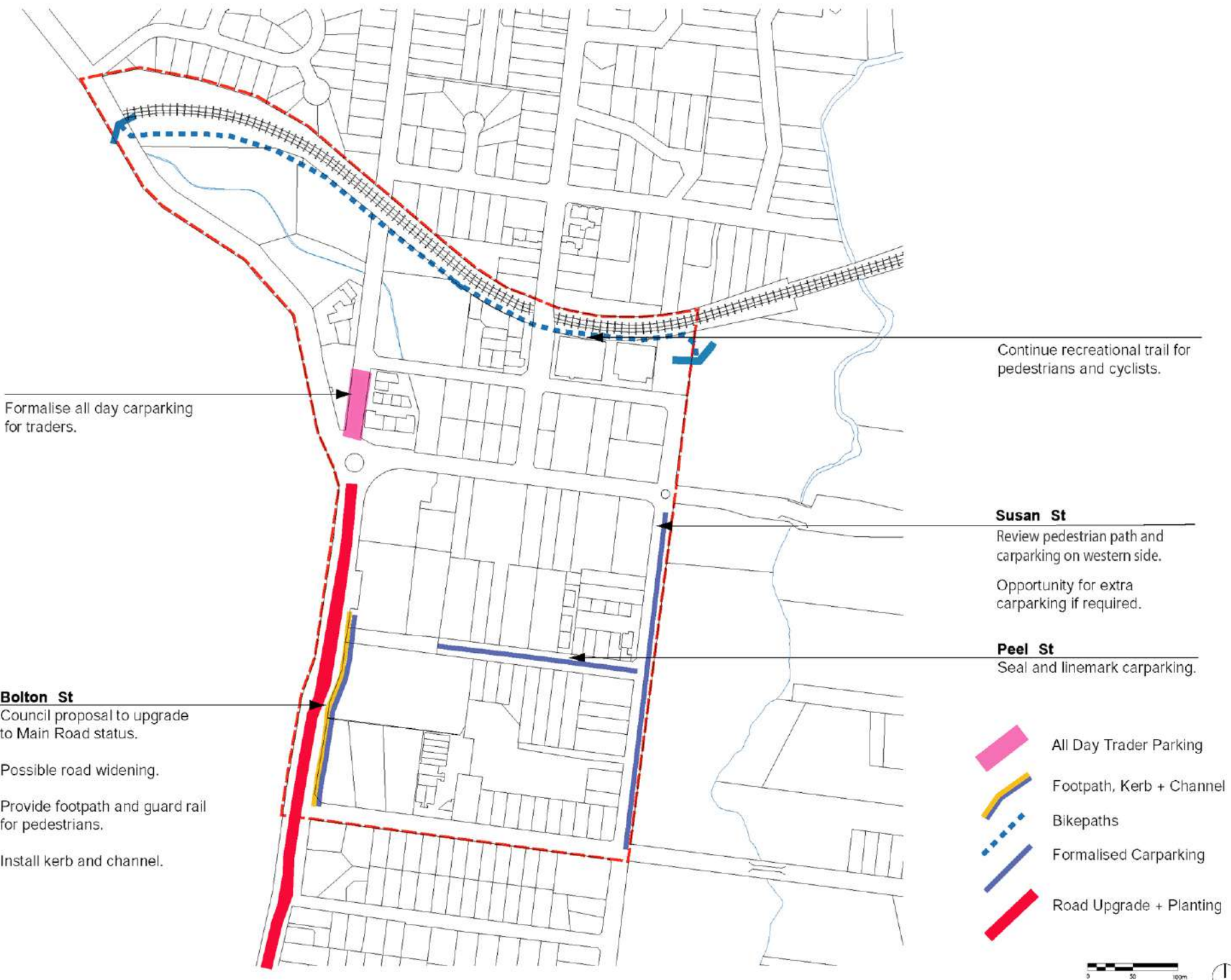
Install kerb and channel.

Provide footpath on east side for school children away from traffic.

Provide picnic tables and seating on parkland side and strengthen planting (trees and grasses) to park interface.

- Revegetation Works
- Footpath + Guardrail
- Bikepaths
- Formalised Carparking
- Road Upgrade + Planting





Formalise all day carparking for traders.

Bolton St
 Council proposal to upgrade to Main Road status.
 Possible road widening.
 Provide footpath and guard rail for pedestrians.
 Install kerb and channel.

Continue recreational trail for pedestrians and cyclists.

Susan St
 Review pedestrian path and carparking on western side.
 Opportunity for extra carparking if required.

Peel St
 Seal and linemark carparking.

-  All Day Trader Parking
-  Footpath, Kerb + Channel
-  Bikepaths
-  Formalised Carparking
-  Road Upgrade + Planting



Eucalyptus rubida
Candlebark



Eucalyptus polyanthemus
Red Box



Eucalyptus melliodora
Yellow Box



Eucalyptus tricarpa
Red Ironbark



Acacia melanoxylon
Blackwood



PLANT SCHEDULE

Botanical Name

Common Name

Height at Maturity(m)

Width at Maturity(m)

Botanical Name

Common Name

Height at Maturity(m)

Width at Maturity(m)

Trees

| | | | |
|---------------------------------|--------------|----|----|
| <i>Acacia implexa</i> | Lightwood | 5 | 3 |
| <i>Acacia melanoxylon</i> | Blackwood | 20 | 10 |
| <i>Allocasuarina littoralis</i> | Black Sheoke | 8 | 5 |
| <i>Eucalyptus melliodora</i> | Yellow Box | 20 | 10 |
| <i>Eucalyptus tricarpa</i> | Red ironbark | 20 | 15 |
| <i>Eucalyptus polyanthemus</i> | Red Box | 18 | 12 |
| <i>Eucalyptus rubida</i> | Candlebark | 20 | 15 |

Grasses

| | | | |
|----------------------------|-------------------------|----|-----|
| <i>Dianella longifolia</i> | Pale Flax Lily | .5 | .5 |
| <i>Joycea pallida</i> | Silvertop Wallaby Grass | .3 | 1 |
| <i>Lomandra longifolia</i> | Spiny Headed Mat Rush | .8 | 1.2 |
| <i>Poa labillardierei</i> | Common Tussock Grass | .5 | .5 |

| | | | |
|------------------------------------|-----------------------|----|-----|
| <i>Poa morisii</i> | Soft Tussock Grass | .3 | .3 |
| Low shrub/groundcovers | | | |
| <i>Brachyscome multifida</i> | Cut-leaf Daisy | .4 | 1 |
| <i>Brachyscome diversifolia</i> | Tall Daisy | .3 | .3 |
| <i>Chrysocephalum appiculatum</i> | Common Everlasting | .3 | 1 |
| <i>Chrysocephalum semipapposum</i> | Clustered Everlasting | .6 | 1.5 |
| <i>Correa reflexa</i> | Common Correa | .8 | 1.5 |
| <i>Correa glabra</i> | Rock Correa | 3 | 3 |
| <i>Dilwynia cinerascens</i> | Grey Parrot Pea | .8 | 1.5 |
| <i>Hardenbergia violacea</i> | Happy Wanderer | .5 | 1.5 |
| <i>Kunzea ericoides</i> | Burgan | 4 | 2 |
| <i>Pultenaea gunii</i> | Golden Bush Pea | .8 | .5 |



Acacia implexa
Lightwood



Correa reflexa
Common Correa



Lomandra longifolia
Spiny Headed Mat Rush



Hardenbergia violacea
Purple Coral Pea



Chrysocephalum appiculatum
Common Everlasting



Bridge Street Zone
 Trees - road reserve street trees
Eucalyptus rubida (Candlebark)

Trees - private planting zones
Eucalyptus rubida (Candlebark)
Acacia implexa (Lightwood)
Eucalyptus tricarpa (Red Ironbark)

Grasses - road reserves
Dianella longifolia
Joycea pallida
Lomandra longifolia
Poa labillardierei
Poa morisii

Low shrub/groundcovers - private zones
Hardenbergia violacea
Correa glabra
Correa reflexa

Parkland Zone
 Trees - road reserve street trees
Eucalyptus rubida (Candlebark) Dominant Planting
Eucalyptus polyanthemos (Red Box)
Eucalyptus tricarpa (Red Ironbark)

Trees - private planting zones
Eucalyptus rubida (Candlebark)
Acacia implexa (Lightwood)
Eucalyptus tricarpa (Red Ironbark)

Grasses - road reserves
Dianella longifolia
Joycea pallida
Lomandra longifolia
Poa labillardierei
Poa morisii

Low shrub/groundcovers - private zones
Brachyscome multifida
Brachyscome diversifolia

Creek Zone
 Trees + Shrubs
Eucalyptus camaldulensis (River red Gum)
Allocasuarina littoralis (Black Sheoke)
Callistemon sieberi (River Bottlebrush)

Grasses - Creek Bank / Erosion Control
Carex appressa
Joycea pallida
Poa labillardierei
Poa morisii

Industrial Street Zone
 Trees - road reserve street trees
Acacia implexa (Lightwood)
Eucalyptus polyanthemos (Red Box)

Trees - private planting zones
Acacia implexa (Lightwood)
Eucalyptus melliodora (Yellowbox)
Eucalyptus polyanthemos (Red Box)

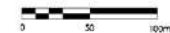
Grasses - road reserves
Dianella longifolia
Joycea pallida
Lomandra longifolia
Poa labillardierei
Poa morisii

Low shrub/groundcovers - private zones
Hardenbergia violacea
Chrysocephalum appiculatum
Correa reflexa

Major Road Zones
 Trees - road reserve street trees
Eucalyptus melliodora (Yellowbox)
 Spacing to be increased from other zones
 to allow for viewlines

Trees - private planting zones
Allocasuarina littoralis (Black Sheoke)
Acacia implexa (Lightwood)
 Increased residential property,
 Screening opportunity

Grasses - road reserves
Dianella longifolia
Joycea pallida
Lomandra longifolia
Poa labillardierei
Poa morisii





Provide guard railing along existing footpath.

Additional planting along embankment to strengthen gateway treatment whilst retaining views into the site from the road.

Provide all day trader parking in closed section of Brisbane St.

Major Entrance to Bridge Street precinct. Key location of low wall with Precinct Signage.

Identified potential location for public art (Long term)

Construct a shared pathway for recreational cyclists and pedestrians as an alternative route to Bridge St. Link pathway to existing parkland trails in Meruka Park and Alistair Knox Park.

Old Hat Factory Garden
Investigate options for acquisition or leasing of land south of creekline for public use.
Investigate interest in developing a Friends group to help plant and maintain area.
Provide boardwalk style footpath down into site and tables for picnics.

Undertake weed control and revegetation along Brisbane St embankment.

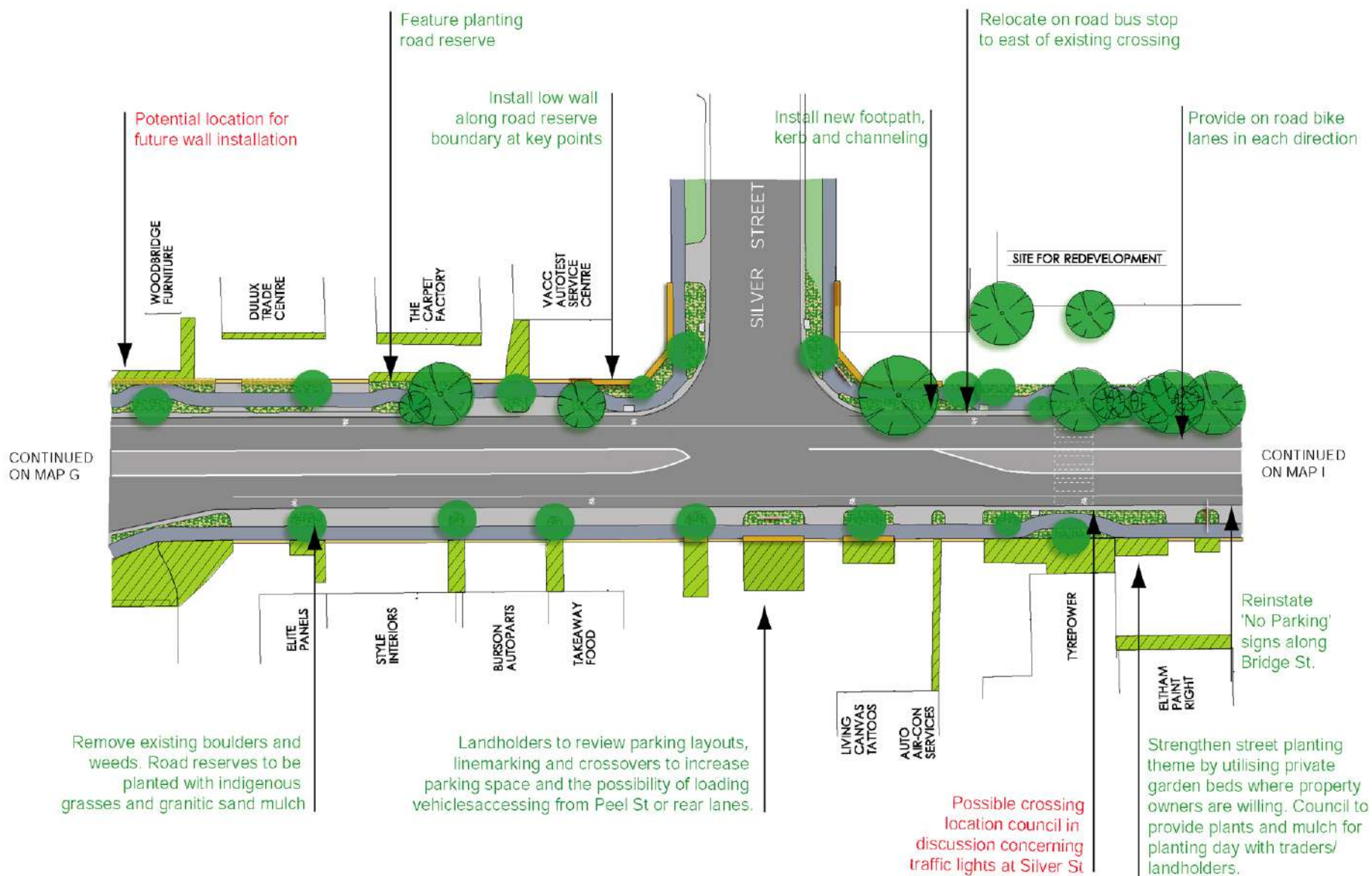
OBJECTIVES













1. To improve the functionality of the Bridge St Employment Precinct for the benefit of businesses, customers and the local community.
2. To improve the appearance of Bridge St as a gateway to Eltham.
3. To improve the visual integration of the precinct with the surrounding environment.

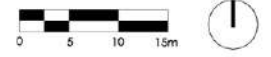


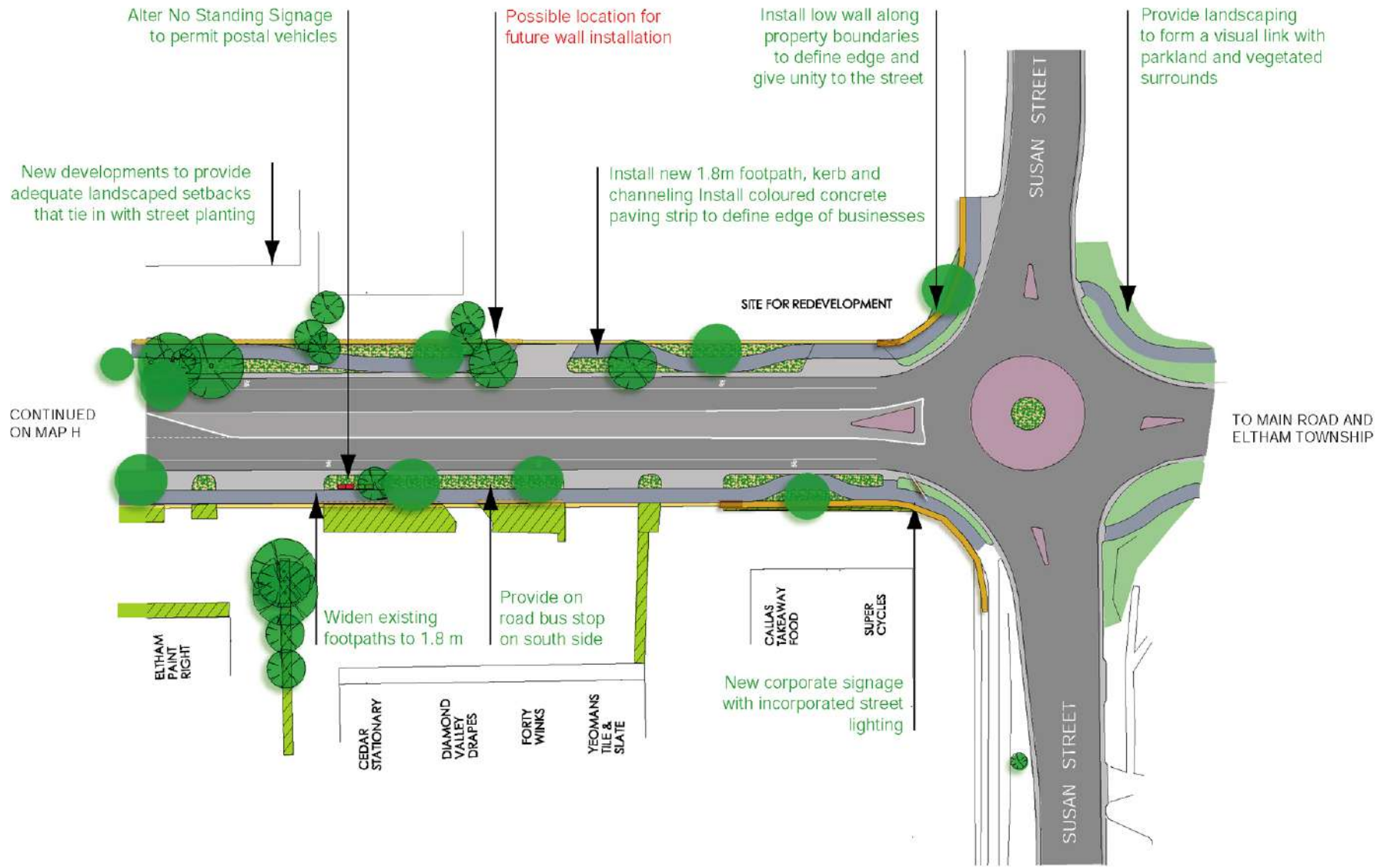
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|---------------------------------|------------------------------------|--|
| Short Term Works - 1-5 years | Proposed low concrete wall | Formal seating locations |
| Long Term Works - 5-10 years | Possible location for future walls | Proposed tree Location subject to powerline relocation and liaison with traders and landholders |
| Footpath | Existing road reserve | Existing tree |
| Proposed coloured concrete edge | Existing private landscaped areas | Proposed combined signage |
















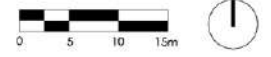


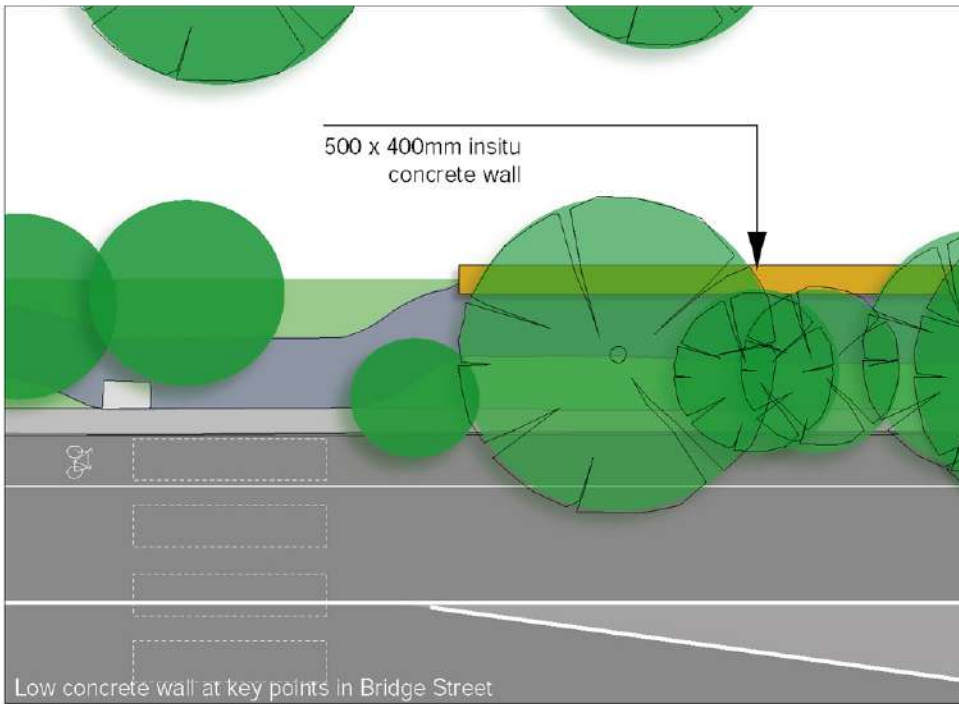
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- | | | |
|---|--|---|
|  Short Term Works - 1-5 years |  Proposed low concrete wall |  Formal seating locations |
|  Long Term Works - 5-10 years |  Possible location for future walls |  Proposed tree |
|  Footpath |  Existing road reserve |  Location subject to powerline relocation and liaison with traders and landholders |
|  Proposed coloured concrete edge |  Existing private landscaped areas |  Existing tree |
| | |  Proposed combined signage |





BRIDGE STREET Feature Wall Treatment

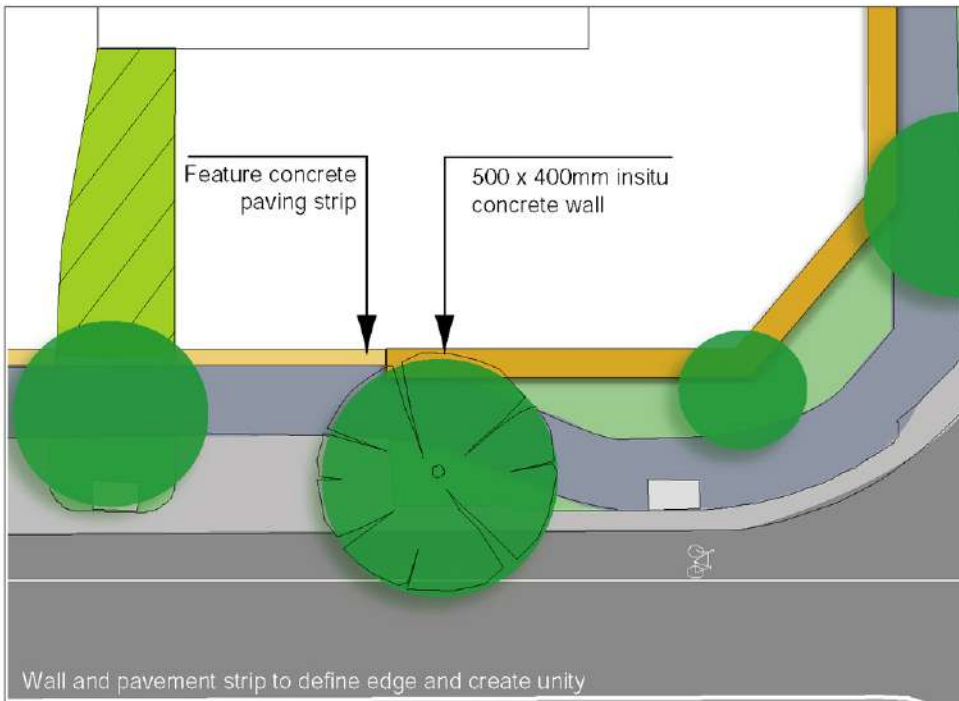
- A low concrete rendered wall (500mm high x 400mm wide) on the road reserve boundary will be installed at strategic locations between the two roundabouts.
- The wall is intended to provide unity to the precinct and define the edge of the road reserve.
- The wall is initially located at key intersections to double as an entrance feature.
- The wall can function as informal seating and retain soil where necessary on the northern side of Bridge St.
- The low concrete wall will be installed at key intersections as a priority, with options for future locations in front of private businesses to continue unity within the street.



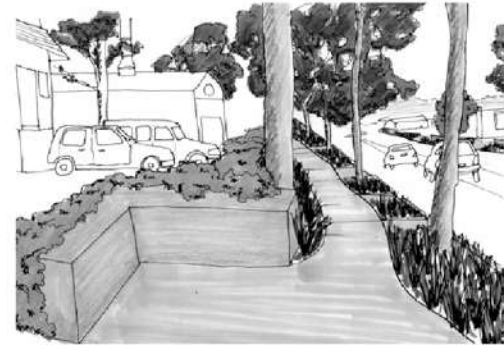
An example of in situ concrete walling



Wall design at Diamond Creek



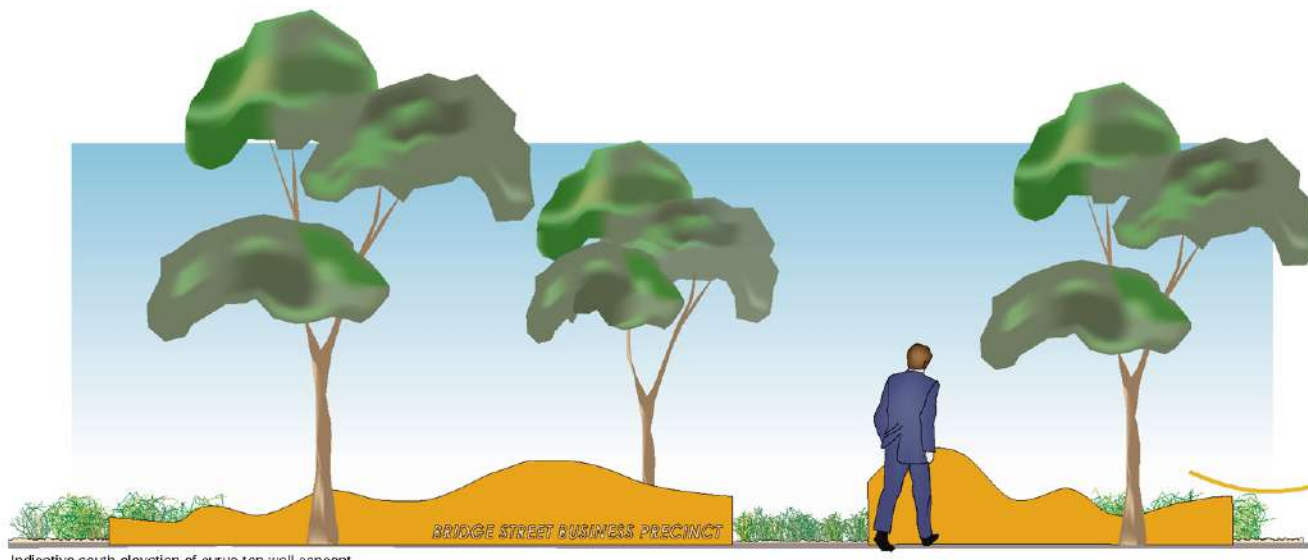
- Where this wall does not occur, a narrow strip of feature paving in line with the wall will further emphasise the street edge and continue the connection between the walls and other elements within the street. It is proposed that this stripe coloured concrete, possibly an ochre yellow to create a contrast from the street while connecting with existing themes within Eltham.



Concept sketch of walls along Bridge Street

- A priority for Bridge street is to implement the undergrounding of overhead powerlines. This will greatly reduce visual clutter. A 1800mm wide concrete footpath will weave its way through existing gums and be edged by low groundcovers and grasses. The existing concrete footpath will be modified to weave through existing and proposed tree plantings.





Indicative south elevation of curve-top wall concept

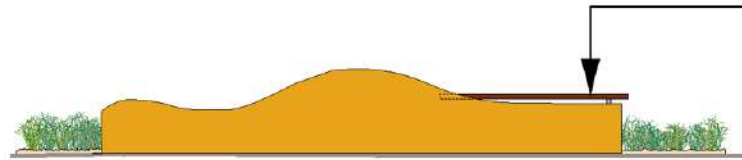


Existing retaining walls in the Old Hat Factory garden



South to North elevation of curve-top wall concept

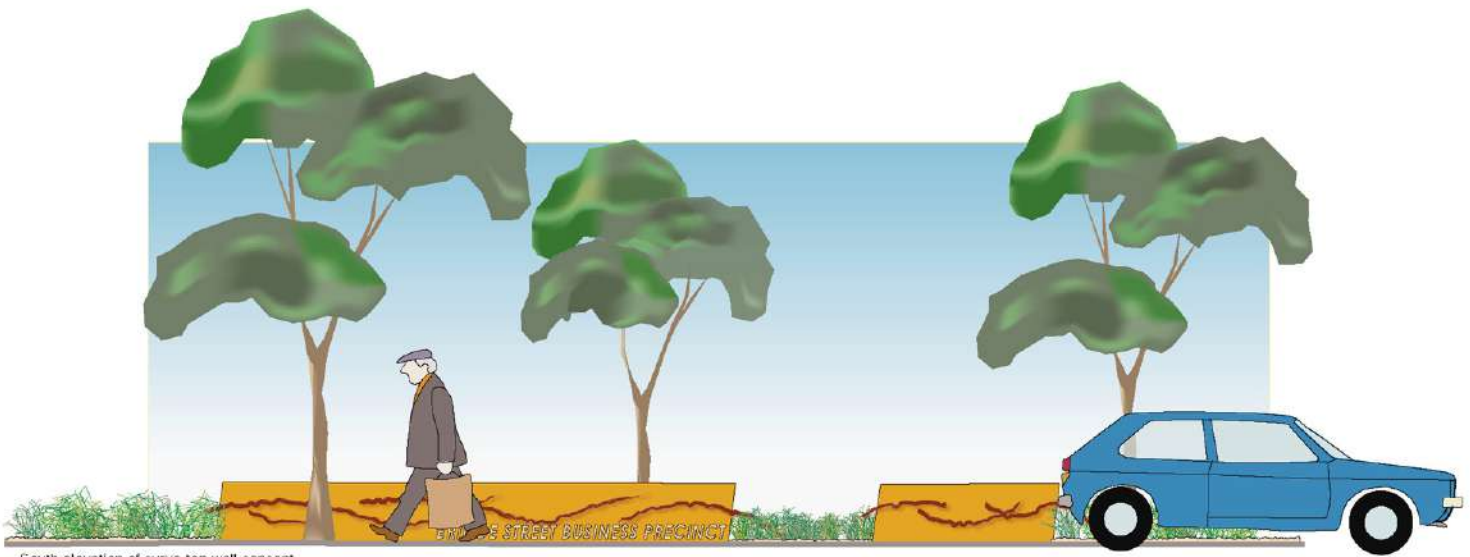
- The form of this walling option is an interpretation of the curving walls evident in the Old Hat Factory garden.
- Due to limited space in the road reserve the curved form has been translated to the top of the walls.
- These walls are intended to be formed, insitu concrete walls coloured with oxide during the mixing process. There are also opportunities for inserting precinct branding text into the walls.
- This curve topped wall would allow for high and low points as it occurs in different locations throughout the street and would offer vertical variation.
- This form of wall would differentiate the employment precinct from other zones within Eltham yet would still offer similarity of treatment with other civic spaces.



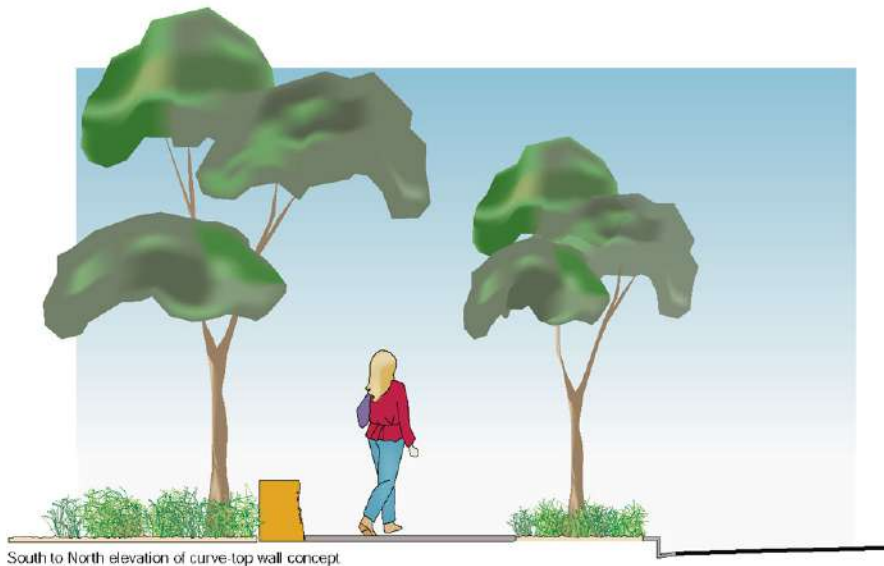
The wall in itself offers opportunities for informal seating, however key points for formal seating such as bus-stops or food outlets can house specialist seating mounted into the top of the wall.



Blades or knobs can be fitted to sections of the wall without landscaping immediately adjacent to deter skateboarders.



South elevation of curve-top wall concept



South to North elevation of curve-top wall concept

- This wall option is based on a simple coloured concrete insitu wall. Possible colours include an ochre colour which will create a link with the other elements within the shire yet will contrast from other elements within the street, making it a dominant feature that contributes to the gateway treatment of Bridge Street as a whole
- To add interest and an industrial edge to this concept, metal reinforcing material as found on the Old Hat Factory site, is included in the side of the wall.
- Curves and twists of this untreated rusting material will be attached to the wall during the forming process. This design will be exposed by sandblasting the face of the wall.
- This treatment plays upon the industrial nature of the precinct and provides an element that is constantly under change due to the effects of rust and it's impact down the wall's face.



The wall in itself offers opportunities for informal seating, however key points for formal seating such as bus-stops or food outlets can house specialist seating mounted into the top of the wall.



Existing metal work - Rusted Metal Reinforcing



The effect of rusted metal on concrete - feature treatment for wall concept



Utilising paving paint and stencils for pavement detailing



Potential wall detailing - indented moulded wall text and painted 'shadow'

3.3 SIGNAGE

Advertising signage is an important component for commercial and industrial buildings as they serve to identify, conform, direct, regulate and interpret information. At present, applications for Advertising Signage is subject to the requirements of Category 2 Office and Industrial – Clause 52.05 Advertising Signage. There is no Schedule to the Advertising Signs at Clause 52.05.

Signage or the consideration there of is mentioned in Council’s Signage Policy at Clause 22.09, however there are no specific guidelines for industrial areas within the Shire;

Consequently, it is important that each building or group of buildings has a consistent and comprehensive sign approach. Placement, scale and legibility should be consistent with the development overall and subordinate to architectural and landscape elements.

For detailed signage options and imagery see Maps M and N.

- Adopt Business Identification Signage in replace of ‘A Frames’
- Review past approvals and remove “illegal signage”
- **Public Land** : Install Bus Identification signage
- **Private Land**: Encourage signage attached to building facades while reducing external signage removed from building.

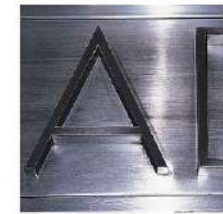
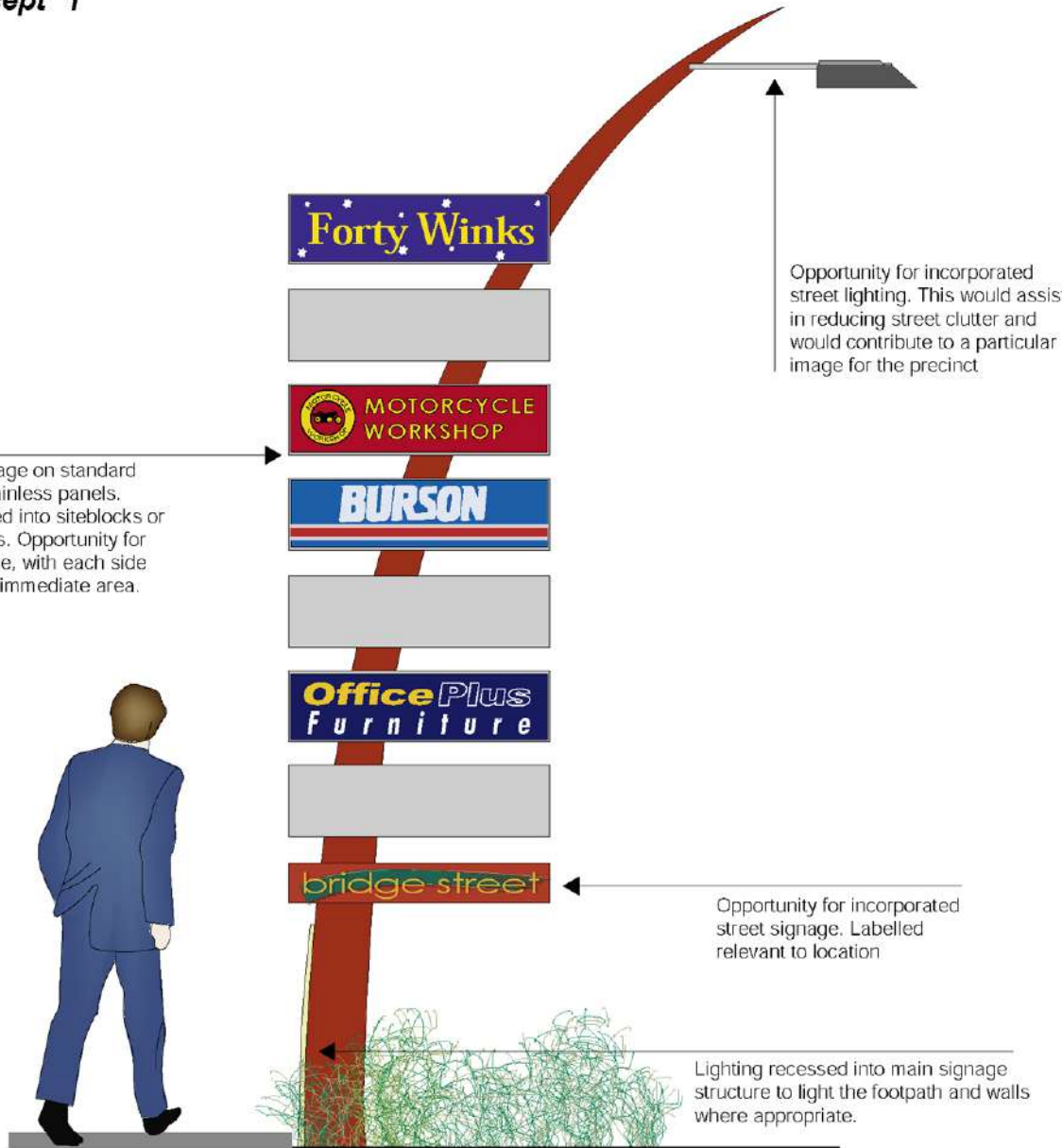
Two potential signage concepts have been suggested in Maps M and N that incorporate industrial materials of the area create an individual identity for the business area. This co-located signage is intended to standardise on-street business advertising and reduce street clutter. These signs would allow for individual business colours and can be updateable as businesses change. There are also options for incorporating street and pedestrian lighting as part of signage structures. Such signage would be first installed in Bridge Street, with other areas being implemented at a later stage

The opportunity exists for the general area to be improved through a coordinated approach to signage. This should not only address signage associated with the private realm but should also provide for the opportunity for the provision of effective signage in the public realm.



PRECINCT SIGNAGE Concept 1

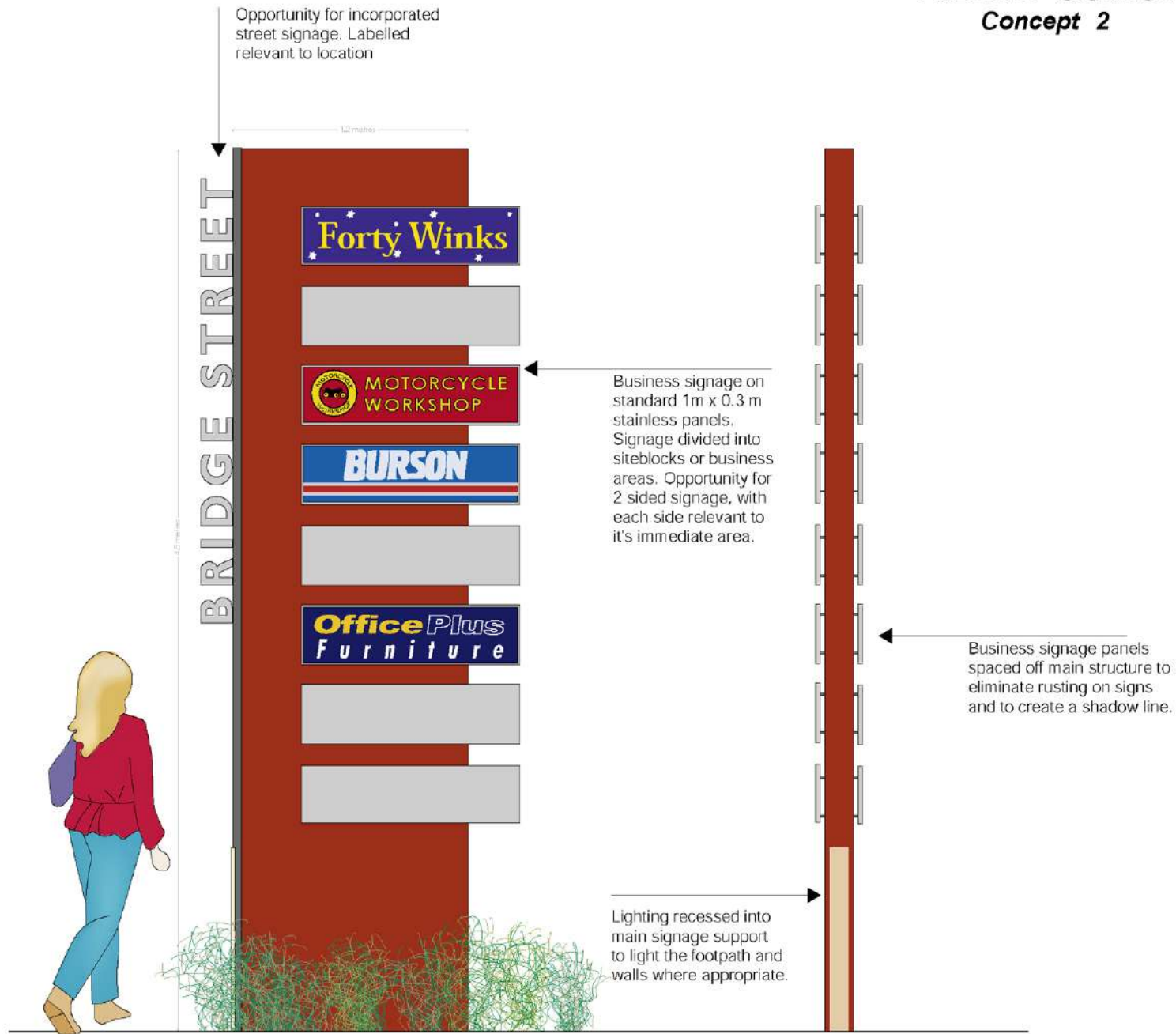
Business signage on standard 1m x 0.3 m stainless panels. Signage divided into siteblocks or business areas. Opportunity for 2 sided signage, with each side relevant to it's immediate area.



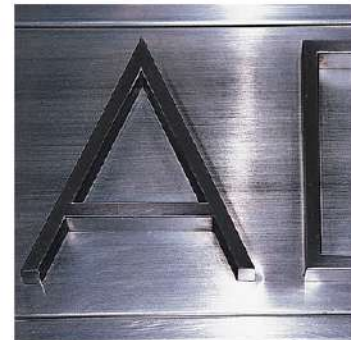
- This signage concept is an arc form that incorporates street and path lighting while providing a distinct identity and clear signage solution.
- The main support arc is formed steel treated with a rusting surface. This materiality touches on the industrial nature of the precinct while using colours that fit well into the surroundings.
- Stainless steel is utilised as a base for individual business signage as a contrast and also to ensure that signs are not damaged by rust.
- The signage incorporates street signs that further separate the precinct and work to define an identity that is independant from other zones within Eltham



PRECINCT SIGNAGE Concept 2



This signage concept is a tall flat structure clad in corten steel panels that appear rusted. This materiality touches on the industrial nature of the precinct while using colours that fit well into the surroundings. Path lighting is inserted into the main structure and it is anticipated that the signage would be uplit with ground mounted lights. Individual stainless steel business sign panels are attached on spacers to protrude beyond the bulk of the structure. The signage incorporates stainless or galvanised street sign lettering that further identifies the precinct.



3.4 LAND USE AND PLANNING

Although the Bridge Street Business Precinct is identified as one of three core industrial areas in Council's MSS, it does in fact contain a more diverse range of uses than the current Industrial zoning would suggest.

Of the 220 businesses currently operating within the Bridge Street Business Precinct, only 55% are industrial in use, with 51% of these being motor vehicle repair companies. Conversely, about 45% of total businesses within the Precinct represent service-based uses containing retail, offices and medical uses as well as a number of vacant or unsigned premises. This would suggest that the likelihood of the Precinct remaining as a primarily Industrial area is under serious doubt. In addition, infiltration of less expansive industrial land by uses that should be in the Town Centre has occurred and it may appear that opportunities to locate in the Town Centre have been limited.

Reasons as to why this is the case are attributable to both the broad scale changes to the nature of industry as well as the specific constraints that exist within the Precinct itself. For example, many sites within the Precinct are occupied by ageing and run down buildings that would require considerable investment to ensure compliance with current standards. Furthermore, complex lot configurations and sizes, relatively poor access to major transport routes and arterial roads, sloping land form, the presence of nearby residential uses and generally low amenity levels, all act as a major disincentive to attracting industrial uses.

As a result, future investment in industrial uses is unlikely to occur on many of these as the risk to investors of attracting a tenant who is prepared to pay sufficient rent to fund the cost of redevelopment is too great. Simply, new industrial tenants will not pay sustainable rents to locate in premises that are heavily constrained for industrial use. Therefore, the blanket application of all land in the Bridge Street Business Precinct within an Industrial Zone, will ultimately result in the inefficient use of land that is likely to become under utilised or even vacant.

The industrial sector is recognised as an important component of Nillumbik's economy as it provides a number of jobs and investment opportunities. This is recognised in Council's Municipal Strategic Statement (MSS) which expresses the need to retain the existing industrial zoned land within the Shire, including that within the Bridge Street Business Precinct. However, the reasons for doing this need to be reviewed regularly as the elements that influence land use demand are dynamic.

In addition there are a number of sound planning reasons to support the rezoning of parts of the precinct for Business purposes. For example, with the combination of existing uses, high exposure in the order of some 19,000 vehicles per day and good local access, Bridge St enjoys an ideal location suitable for warehouse sales, show rooms, offices and professional suites. This is already evident by the number of uses that rely on passing trade that have established themselves along Bridge St in recent times.

Also, business uses can provide a greater opportunity to implement urban design objectives relevant to the role of Bridge Street as a gateway create attractive interfaces with residential and recreational areas.

3.4.1 Zoning Recommendations

Given Council's imperative for local job retention and creation, it may be appropriate to consider parts of the precinct being in a Business Zone. This would allow for a mix of industrial and office uses, both of which will facilitate employment-generating uses.

Whilst the implementation of urban design frameworks can support/achieve some economic development the introduction of a Business zone in certain parts of the Bridge Street Business Precinct will not only better reflect the existing uses, but also reconcile the different opportunities and constraints that currently surround the study area.

Underpinning this is the fundamental need to ensure that the Precinct can adapt to changing conditions so that it remains a long term, economically viable area.

However, the consideration of the future uses/zones in the Bridge Street Business Precinct must be assessed within the context of the broader Structure Plan work being undertaken by Nillumbik Shire Council.

3.4.2 Policy Recommendations

The Bridge Street Business Precinct Urban Design Framework makes a number of recommendations which require the introduction of new policy or in some cases, amendment to existing policy. The policy implications identified are outlined as follows:

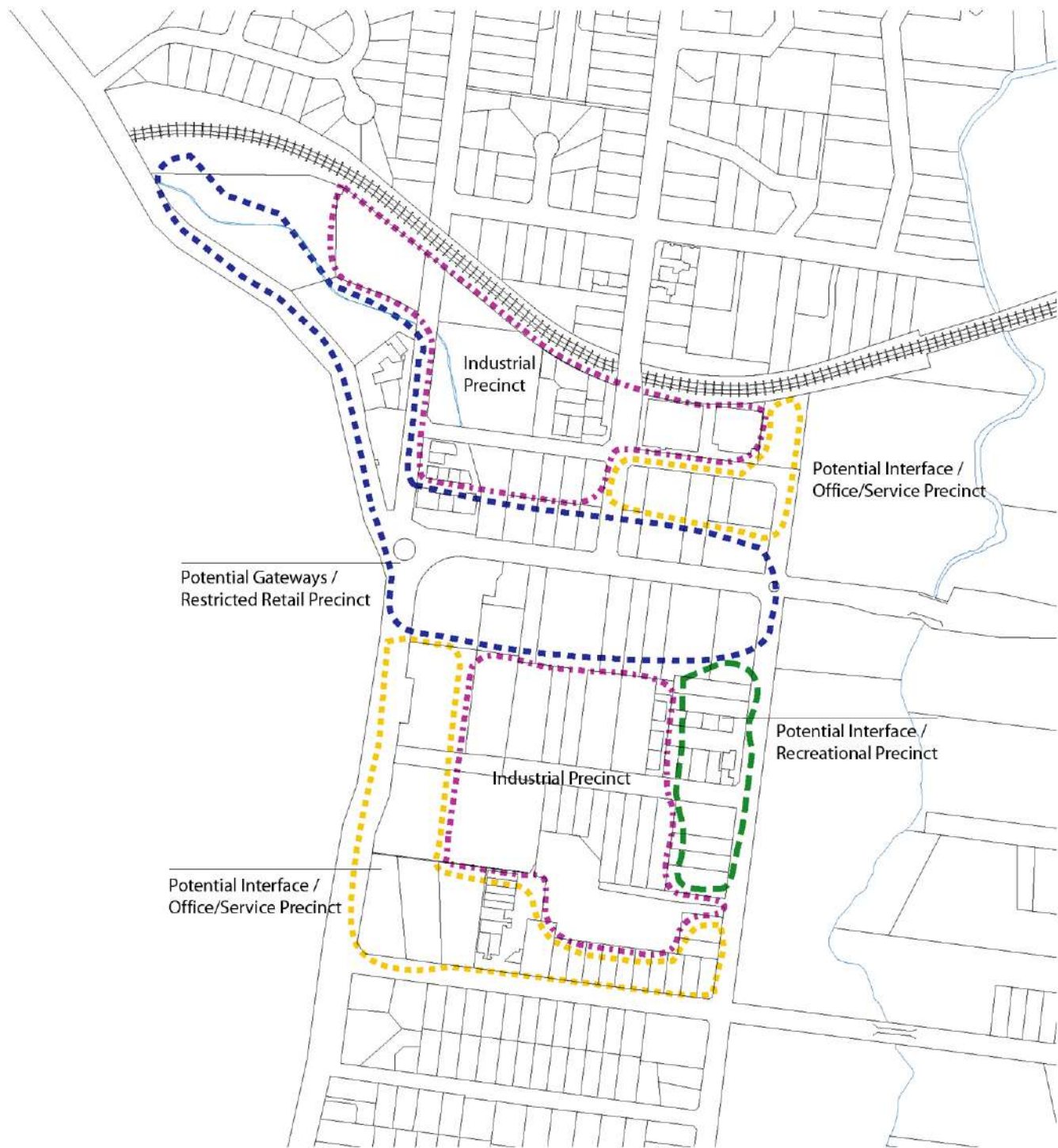
Review Clause 22.08 – Industrial Areas Policy, to consider amending, as part of the Eltham Major Activity Centre Structure Plan, to allow for the consideration of uses associated with any Business zoning proposal.





Incorporate the Bridge Street Business Precinct Design and Development Guidelines included in this report, into the Nillumbik Planning Scheme. It is recommended that these guidelines be introduced into the Scheme as a Local Planning Policy. Within this policy, specific *Business Precincts* should be identified in accordance with Map O. The purpose of identifying these precincts is to encourage particular use and development outcomes within the Precinct that respond positively to their particular contexts, whether it be a main road frontage, residential dwellings, park land or the railway line.

3.4.3 Further Work Recommended

It is acknowledged that in the context of Council's Economic Development Strategy and Structure Plans for Eltham and Diamond Creek that all the Shire's existing industrial areas will be reviewed. Such a review should identify appropriate strategic directions within the local and regional context, and with regard to suitable future uses in the context of changing trends in the industrial sector. This will ensure that Council's overall objectives for local employment and industry are achievable and supported by appropriate zoning controls.





-  Potential Gateways / Restricted Retail Precinct
-  Potential Interface / Office/Service Precinct
-  Industrial Precinct
-  Potential Interface / Recreational Precinct
(eg. gymnasiums, indoor sports facilities etc.)



3.5 BRIDGE STREET BUSINESS PRECINCT DESIGN AND DEVELOPMENT GUIDELINES

Policy Basis

Industry is an important element of any community. It creates employment generates income and promotes growth. The planning scheme must provide for the continued operation of existing industry and for the creation of new industry. It must provide for the coexistence of industrial and residential and other sensitive uses.

Apply the State Planning Policy Framework provisions relating to Industry (Clause 17.03) and Design and Built Form (Clause 19.03) to local circumstances.

Intent

- To promote and facilitate the visual upgrading of the Bridge Street Industrial Precinct with a comprehensive landscape program
- To maintain and enhance industrial areas that:
 - Co-exist harmoniously with adjoining residential and other land-use precincts and do not compromise the amenity of surrounding areas.
 - Maximise employment opportunities.
 - Are environmentally sustainable, particularly in relation to minimal air, water, land, noise and visual pollution.
 - Offer a high level of amenity and adequate services and facilities for workers.

Policy

- To ensure that sites within the Bridge Street Business Precinct are neatly maintained and have an acceptable appearance to convey an image of pride and confidence in local businesses.
- To further enhance links and improve links between the Bridge Street Business Precinct with the Eltham Town Centre
- To ensure the operation of employment sites are efficient and safely interact with the road network.
- To ensure appropriate standards of health, safety and amenity on industrial sites.

This will be achieved by:

- Requiring a site analysis and design response in accordance with the requirements of Clause 19.03 Design and Built Form to be submitted prior to the consideration of an application for buildings and works;
- Requiring a detailed assessment of existing and proposed car-parking in accordance with the requirements of Clause 52.06 - Carparking and Clause 52.07 - Loading and Unloading of Vehicles

1. STREET CONNECTION

Objectives

- To ensure that new buildings relate positively to the adjacent street and surrounding public spaces.

Standards

- Pedestrian entries into the building must be designed to be clearly visible and directly face the street.
- The street facade and internal layout of the ground floor must be designed to facilitate visual and physical access between the building and the adjoining public realm. This may be achieved by providing clear glazed windows at ground level to allow for casual surveillance of adjoining external spaces.
- The layout of the ground floor should be designed to ensure that 'people intensive' uses are located at ground level.
- Building frontages must be parallel with street boundaries to create and/or reinforce a strong built form definition of the street.
- Ground level facades of new buildings on these corners should address both streets and must be designed to facilitate visual and physical access between the building and the adjoining public realm.

2. PUBLIC SAFETY & SECURITY

Objectives

- To ensure that new buildings enhance safety and security within the site and surrounds.

Standards

- External public and communal spaces such as building entries, parking areas and pedestrian paths, must be well lit and clearly visible to allow for casual surveillance.
- New developments must provide a clear boundary between public and private spaces to avoid ambiguity. This may be achieved using landscaping and/or built edges to define site boundaries.

3. PEDESTRIAN ACCESS

Objectives

- To enable the use of buildings by all people.
- To promote pedestrian flow, safety and amenity.

Standards

- Pedestrian access points must be separate from vehicle access points.
- Buildings should be designed to be wheelchair accessible to front entrances.

- A continuous accessible route must be provided between disabled parking spaces and the entrance of the premises they serve.
- Key access routes into buildings must have good visibility and be well lit.
- Safe and convenient access must be provided throughout all new developments including:
 - the main entrance and exits
 - all on site 'public' areas, and
 - into all lifts and car parks.

4. BUILDING SCALE & HEIGHT

Objectives

- To ensure that the height of new buildings responds to the desired scale and character of the street and local area.
- To allow reasonable daylight access to all developments and the public domain.
- To ensure that the height and form of new buildings maintains and where appropriate, enhances the amenity of existing residential uses.

Standards

- Buildings on Bolton St may not exceed 9.0 metres above natural ground level at grounds highest point unless it is setback a minimum of 6 metres from the front boundary.
- For buildings in Bolton St, consideration must be given to the height and bulk of the building when viewed from the east. (Main Rd Eltham)
- New buildings on those sites at the corner of Bridge St/Bolton St and Bridge St/Susan St may be built one storey higher than the predominant Bridge St building height to emphasise their gateway status.
- New buildings on these sites should emphasise their gateway status by being built to the street alignment, using taller, distinctive architectural elements and incorporate interesting roof top forms, which add interest and variety to the skyline.
- In other areas the overall height of new development may exceed the dominant building height of the area if the site is able to accommodate a taller built form without causing off site amenity impacts.

5. BUILDING APPEARANCE

Objectives

- To ensure that buildings provide visual interest from the street.

Standards

- Building facades should incorporate relief to ensure visual interest. This may be achieved by using modulated forms, projections, recesses and voids.
- Buildings should incorporate interesting roof top forms, which add interest and variety to the skyline.
- Avoid highly reflective materials and colours on external walls that face the public realm, in particular opposite residential areas. Sheer curtain walls or other expanses of reflective glass are discouraged. Building colours should be neutral or subdued, where they are opposite an adjoining residential area or park.

6. ENERGY EFFICIENCY

Objectives

- To maximise the energy efficiency of all new buildings by minimising the use of energy and natural resources.

Standards

- Where the shape of the block permits, face the long side of the building to the north.
- Control summer heat gain from solar penetration by minimising east and west facing glass and shading glazing (including roof lights) with external louvres or blinds, verandahs, projecting balconies, pergolas or planting.
- Insulate the roof, ceiling and walls so that it protects the internal environment from the sun in Summer and retains the heat in Winter.
- New buildings should not unreasonably reduce the energy efficiency of existing, adjoining buildings
- Apply the standards outlined in the Green Building Council of Australia's 'Environmental Rating Scheme – Commercial Buildings' (due for release in July 2003).

7. SITE FACILITIES

Objectives

- To ensure that all site facilities are adequate, convenient and obscured from the public realm.

Standards

- Rooftop servicing should be designed and located on the building so as not to detract from the appearance or be visible for pedestrians in the street.
- External services should be designed to be integrated into a building's architecture and not be visible from the street.
- All lift overruns should be concealed within the roof design of new development
- Bin and recycling facilities should be located for convenient access by users and collection vehicles
- Garbage and recycling bin enclosures should be adequate in size, durable waterproof and blend in with the development to avoid visual clutter.
- Mailboxes must be located close to the pedestrian entry for convenient access by residents and as required by Australia Post.

8. LANDSCAPING

Objectives

- To enhance the appearance of the development and integrate with the landscape character of the surrounding area. To reinforce and enhance the Bridge St boulevard theme.
- To enhance and improve the micro-climatic conditions of the development.

Standards

- A high standard of landscape treatment must be provided in the front setback where buildings are setback from the street. This must include a row of large canopy trees, which complement the boulevard theme and frames the building (*Species selection must be consistent with Maps D + E*).
- Where practical, car-parking areas must be landscaped with appropriate canopy trees and maintained in accordance with an approved landscape plan.
- Where buildings are setback from Bridge Street, a 2-metre landscape zone must be provided along the front street boundary. In all other streets, a 1.5m landscape zone must be provided along the street boundary where building setbacks permit.

- Where practical, the rear of sites should also be landscaped according to the species recommendations in Maps D and E.
- Construct a low wall along the Bridge Street road reserve boundary to create a sense of unity and define private and public spaces. All paved surfaces and other hard standing areas, including car parks, are to be constructed with dark coloured concrete or bitumen, coloured patterned concrete or brick paving to achieve visual interest and compatibility with the character of the area.

9. CAR PARKING & VEHICLE ACCESS

Objectives

- To ensure that car parking areas and driveways contribute to the functioning, safety and appearance of new development.
- To ensure safe and convenient vehicle movements and connections within the site and to the street network.
- To ensure that the design of parking and access areas is practical and attractive and can be easily maintained.
- To ensure that car parking provision for building occupants and visitors is sufficient.

Standards

- The number of vehicle crossovers must be kept to a minimum.
- Vehicle access points must be separate from pedestrian access points.
- Loading bays should be clearly separated and screened from pedestrian areas and must be concealed from the site's primary frontage and street corners.
- Tandem spaces should be avoided.
- Car parking areas must have well-defined vehicle entry points, preferably for one-way traffic flows and enabling vehicles to exit the site in a forward direction onto abutting roads.
- Car parking areas must be designed, drained, graded and formed in accordance with good engineering practice and, where appropriate, in accordance with an engineering construction plan submitted to and approved by the responsible authority.
- All parking areas, including entry and exit points must be well lit.
- Vehicle access should be provided from secondary streets, rights of way or laneways, where these are available and where width is sufficient.
- Prior to the preparation and implementation of a car parking precinct plan, new developments are required to provide adequate car parking on site.

10. ADVERTISING SIGNS

Objectives

The objectives of this policy are:

- To ensure that sign clutter is avoided or reduced in order to maintain or enhance the built and natural environment and avoid unnecessary duplication of signage.
- To provide for the orderly display of signs.
- To ensure that signs do not cause loss of amenity or adversely affect the natural or built environment or the safety, appearance or efficiency of a road.
- To ensure that advertising signs provide appropriate and effective identification of businesses or other land uses.
- To ensure that advertising signs compliment the amenity and streetscape/landscape character of the surrounding area, particularly in sensitive and prominent landscape areas, residential areas and along main roads.
- To ensure that advertising signs do not detract from the appearance of the building on which they are displayed.
- To ensure that the size and height of an advertising sign is compatible with the scale of the building or the site on which they are displayed, the surrounding streetscape and the size and scale of other signs in the area.

Policy

- Signs should be designed to be proportional to the scale of the building.
- Signs should not visually dominate the area of building walls and parapets or of landscaped surroundings.
- Primary corporate identification signage should be fixed to face of the building.
- For multiple occupancy buildings or sites:
 - grouped signage should be displayed at entry with signs of uniform shape and composition, but allowing for individual 'corporate identity' to be presented; and
 - individual company signage placed consistently on separate buildings.
- Council should provide areas for the coordinated display of signs in place of A-Frame signs.
- Incorporate signage siting guidelines into the planning scheme
- Ensure signage on building facades does not exceed parapet level;
- Limit the use of illuminated signage, especially where it is removed from the building facade;
- Limit the use of pole and rotational signage.

4.0 PRIORITIES AND COSTINGS

IMPLEMENTATION OF FINAL RECOMMENDATIONS FOR BRIDGE STREET BUSINESS PRECINCT

| Key | Timeframe | The strategies and actions have been grouped by locality (ie Bolton St) |
|------------|-----------|---|
| Short term | 0-5 years | |
| Long term | 5-10years | |

| Location | Action | Priority | Notes | Costing | Agency |
|------------------------------|--|------------|--|---------------------------|-----------------------|
| Short Term Priorities | | | | | |
| Whole Precinct | | | | Total: | |
| | • Apply planning controls | Short Term | | N/A | Council |
| Bolton St | | | | | |
| | • Revegetate boundaries of vacant blocks | Short term | 880m2 mulch and plants | At Redevelopment of sites | |
| Silver St | | | | | |
| | • Install kerb and channel | Short term | \$60 per lin.m.x 160m | \$9 600 | Council |
| | • Linemark to increase carpark capacity | Short term | \$2 per lin.m.x 80m | \$160 | Council |
| Susan St | | | | | |
| | • Install kerb and channel | Short term | \$60 per lin.m. x 400m | \$24 000 | Council |
| | • Provide a footpath on parkland side for school children, separate from traffic | Short term | \$80 per lin. m x 400m | \$32 000 | Council |
| | • Provide picnic tables and seating in parkland for traders use | Short term | 2 tables @ \$950 ea., 6 seats @ \$850 ea. | \$7000 | Council |
| | • Strengthen planting (trees and grasses) along park interface | Short term | 800m2 mulch and plants (trees and grasses) | \$19 225 | Council |
| Peel St | | | | | |
| | • Install additional lighting | Short term | \$500 x 4 ex. poles | \$2 000 | Council |
| Brougham St | | | | | |
| | • Seal and linemark carparks | Short term | \$60 per lin.m. x 320m | \$19 200 | Council |
| Sherbourne Rd | | | | | |
| | • Provide a guardrail along narrow footpath | Short term | 140m W beam guardrail | \$43 500 | Council |
| | • Undertake weed removal and revegetate creekline and Brisbane St embankment | Short term | 750m2 weed removal, planting and mulch | \$9375 | Council |
| | • Strengthen planting to Hat Factory embankment north of Sherbourne Rd | Short term | 400m2 planting and mulch | \$4 600 | Council Friends Group |

| | | | | | |
|-------------------------|---|------------|--|------------|---|
| Bridge St – Streetscape | | | | | |
| | <ul style="list-style-type: none"> Conduct feasibility study on provision of underground power lines | Short term | Feasibility Study | Staff Time | Powerline Relocation Committee/ Council |
| | <ul style="list-style-type: none"> Feature planting to roundabouts | Short term | 330 m2 indig. grasses and granitic sand mulch | \$11 000 | Council |
| | <ul style="list-style-type: none"> Feature planting to surrounding road reserve | Short term | 605 m2 indig. grasses and granitic sand mulch | \$20 000 | Council |
| | <ul style="list-style-type: none"> Remove existing volcanic rocks and other roadside hazards | Short term | 2 x 3 hours labour + transport | \$300 | Council |
| | <ul style="list-style-type: none"> Construct low concrete wall to unify street | Short term | 205 Lm insitu coloured concrete detailed wall (500mm x 400mm) | \$72 000 | Council |
| | <ul style="list-style-type: none"> Install seating units to feature wall | Short term | 6 Timber slat bench seats fixed to top of wall | \$3600 | Council |
| | <ul style="list-style-type: none"> Install 200mm coloured concrete paving strip along length of bridge street on both sides | | 250 Lm X 200 mm wide coloured concrete feature strip - | \$2 500 | Council |
| | <ul style="list-style-type: none"> Install new 1800mm wide concrete footpath curving between existing trees with tactile pavers and ramps at crossing points | Short term | 1800mm wide x 100mm depth reinforced x 655m2 Total length: 364m ex. Existing path | \$38 000 | Council |
| | <ul style="list-style-type: none"> Replace damaged kerb and channel along the north side of the street | Short term | \$60 per lin.m. x 150m | \$9 000 | Council |
| | <ul style="list-style-type: none"> Provision of plants and mulch to traders willing to extend streetscape planting into their private garden beds. | Short term | 230m2 mulch and plants | \$5 800 | Landholders Council |
| | <ul style="list-style-type: none"> Arrange combined Corporate Signage | Short term | 6 + signage units - ~\$6500 p/unit | \$39 000 | Landholders Council |
| Bridge St - Traffic | | | | | |
| | <ul style="list-style-type: none"> Landholders to review parking layouts, linemarking, crossovers to increase parking space | Short term | | N/A | Landholders |
| | <ul style="list-style-type: none"> Relocate northern bus stop to the east of pedestrian crossing | Short term | 2 x 8 hour labour + concrete | \$900 | Council |
| | <ul style="list-style-type: none"> Provide onroad bicycles lane in each direction | Short term | linemarking +bike symbols + signage | \$6 000 | Council |
| | <ul style="list-style-type: none"> Reinstate “NoParking ‘ signs with provision for buses and postal vehicles. | Short term | \$175X 20 signs | \$3 500 | Council |
| Other Areas | | | | | |
| | <ul style="list-style-type: none"> Linemark all day trader carparks in the redundant section of Brisbane St. | Short term | \$2.00 per lin.m. x 280m | \$560 | Council |

| Location | Action | Priority | Notes | Costing | Agency |
|-----------------------------|--|-----------|--|------------------------|---|
| Long term Priorities | | | | | |
| Bolton St | | | | | |
| | <ul style="list-style-type: none"> Install kerb and channel | Long term | \$60 per lin.m. x 400m | \$24 000 | Council Vicroads |
| | <ul style="list-style-type: none"> Construct concrete footpath and guardrail | Long term | \$80 per lin. m x 400m + W beam guardrail | \$153 500 | Council |
| Susan St | | | | | |
| | <ul style="list-style-type: none"> Linemark to provide extra carparking on park edge | Long term | \$60 per lin.m. x 400m | \$24 000 | Council |
| Peel St | <ul style="list-style-type: none"> Review pedestrian paths on West side and car parking | Long Term | Cost subject to the outcomes of eth review | | Council/Local business |
| | <ul style="list-style-type: none"> Seal and linemark carparking | Long term | \$60 per lin.m. x 400m | \$24 000 | Council |
| Sherbourne Rd | | | | | |
| | <ul style="list-style-type: none"> Consider purchase or lease of land to the south of the creek at Hat Factory site as a small picnic spot close to food outlets and in the context of the surrounding area due to it's cultural significance | Long term | Approx. 2340m ² | Subject to land values | Council Local Businesses Friends Group |
| Bridge St – Streetscape | | | | | |
| | <ul style="list-style-type: none"> 350m underground powerlines installation | Long term | Approximate cost of total works pending a successful submission attracting funding and at no cost to Council | \$740 000 | Powerline Relocation Committee/ Local Business |
| | <ul style="list-style-type: none"> Consider additional wall installation to further strengthen visual identity of street | Long term | 100 Lm insitu coloured concrete detailed wall (500mm x 400mm) | \$35 000 | Council |
| Bridge St - Traffic | | | | | |
| | <ul style="list-style-type: none"> Remove redundant crossovers and review widths | Long term | 160m ² x demolition, kerb and channel and soil conditioning | \$14 560 | Landholders Council |
| | <ul style="list-style-type: none"> Investigate the opportunity to provide access to loading facilities from rear streets | Long term | | N/A | Landholders Council |
| Other Areas | | | | | |
| | <ul style="list-style-type: none"> Provide a recreational trail for pedestrians and cyclists along the southern side of the railway reserve in conjunction with other studies including Rec Trails and Eltham Structure Plan | Long term | 1800mm wide asphalt x 700m x \$30 | \$37 800 | Council |