

# Diamond Creek Major Activity Centre

STRUCTURE PLAN

FEBRUARY 2020



# DRAFT







ANYTIME FITNESS

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# DRAFT

## 1.0 Introduction

### 1.1 Introducing Diamond Creek

Diamond Creek Major Activity Centre (Activity Centre) is a retail, recreation and transport hub situated in the distinct landscape setting of the Diamond Creek Valley and characterised by a semi-rural character. The activity centre is anchored along Main Hurstbridge Road, which is bisected by the Diamond Creek and adjacent floodpains and serves as a spine and major road corridor through the centre.

Key uses within the centre include the Diamond Creek train station, Diamond Creek retail core, Chute Street commercial strip, Elizabeth Street industrial area, Diamond Creek Reserve, Diamond Valley College and Diamond Creek Community Centre.

### 1.2 What is a Structure Plan?

A Structure Plan sets the Vision for the area until 2030 to manage change and growth. Structure Plans are strongly focused on sustainability and planning for change that is environmentally, socially and economically sound.

A Structure Plan brings together the inputs of a wide range of stakeholders, state agencies and service authorities to articulate a vision and future direction that provide both Council and community certainty moving forward.

### 1.3 Aim and Purpose

The Diamond Creek Major Activity Centre Draft Structure Plan (Draft Structure Plan) has culminated from a review of the *Diamond Creek twenty20 Major Activity Centre Structure Plan and Leisure Facilities Plan* (September 2006). The context for the Activity Centre has changed since the adoption of the Structure Plan in 2006. The review was required to meet the current and future needs of Diamond Creek. The Draft Structure Plan aims to guide future planning and development in the Activity Centre.

The purpose of the Draft Structure Plan is to set an overall vision for the centre and objectives, strategies and design guidelines focused on:

- land use activities and development;
- built form character;
- landscape and place activation;
- movement and access; and
- community and leisure.



## 1.4 Study Area

The study area was defined by Council when the original structure plan was prepared in 2006. It has remained largely unchanged due to the continued sufficient capacity for development and change.

The study area focuses on the shopping centre of Diamond Creek but also includes the commercial, industrial, residential and open space areas generally within 400m walking distance of the town centre. It is bounded by the Diamond Creek to the north, Challenger Street to the south, Main Street to the west, and Main Hurstbridge Road to the east (Figure 1).

## 1.5 Implementation

The Draft Structure Plan will be implemented over the life of the plan to the year 2030 when another structure plan review will be required.

- Short term: 0-2 years – 2020 to 2022
- Medium term: 3-6 years – 2022 to 2027
- Long term: 7-10 years– 2027 to 2030

## 1.6 Reading This Document

Nillumbik Shire Council has prepared the Draft Structure Plan for the Diamond Creek Major Activity Centre (Activity Centre).

The Draft Structure Plan has been shaped by a number of inputs that include, but are not limited to, rigorous background research analysis, review of the Planning and Local Policy Frameworks, site visits and observations within the study area and discussions and feedback from community engagement forums and meetings with other relevant agencies and stakeholders.

The Draft Structure Plan outlines the Vision for the activity centre and identifies Objectives and Strategies that outline how the Vision will be achieved. The Draft Structure Plan is structured as follows:

Parts 1 – 3 of the document outline the study area and provide existing context and key influences facing the area.

Section 4 identifies a Vision, Objectives, Strategies and framework for the activity centre. The strategies and framework are articulated through five themes:

- Land Use Activities and Development
- Built Form Character
- Landscape and Place Activation
- Movement and Access
- Community and Leisure



Section 5 identifies specific precincts within the Draft Structure Plan study area and objectives and strategies required to achieve the preferred character.

Section 6 provides general design guidelines to be applied to all development proposals.





## LEGEND

-  STUDY AREA
-  TRAIN STATION AND RAIL LINE

## DIAMOND CREEK MAJOR ACTIVITY CENTRE PRECINCTS








-  1 CHUTE STREET COMMERCIAL STRIP
-  2 CREEK-SIDE OPEN SPACES, COMMUNITY CENTRE & RECREATION
-  3 ELIZABETH STREET / STATION STREET
-  4 RETAIL CORE
-  5 EAST OF GEORGE STREET, NORTH OF MAIN HURSTBRIDGE ROAD
-  6 EAST OF WENSLEY STREET, SOUTH OF MAIN HURSTBRIDGE ROAD
-  7 SCHOOLS AND RECREATION

Figure 1. Study Area and Sub-Precincts

## 2.0 Context and Key Influences

### 2.1 Strategic Context

Diamond Creek Major Activity Centre (Activity Centre) is located at the fringe of Metropolitan Melbourne, encircled by green wedge to the north, east and south. Its location next to the green wedge reduces its growth rate as compared to major activity centres closer to the CBD.

It is located 25km north west of the Melbourne CBD and 5km from the Eltham and Greensborough Major Activity Centres.

The Activity Centre is accessible via the Hurstbridge rail line, and connected to Eltham Station, Greensborough, Hurstbridge and Mernda via bus services (Figure 2). The Diamond Creek Trail also connects the Activity Centre to Eltham and onwards to the CBD. There are also future plans to connect the trail through to Hurstbridge.

### 2.2 Brief History of Diamond Creek

The area of the Shire of Nillumbik was for centuries home to the Wurundjeri-willam clan of the Woi wurrung speaking people. This is evidenced in a number of archaeological sites across the Shire.

Colonial settlers arrived in the area of Diamond Creek in the 1830's. The township was established after gold was discovered in the area in 1854. It was initially named Nillumbik, which also became the name of the parish. By the time the government proclaimed it as a township in 1866, popular usage of the name Diamond Creek prevailed, and it was officially given this name.

The first bridge across Diamond Creek in this area is near the present Chute Street by the late 1840's. The pattern of early tracks from the 1840's and 1850's created the pattern of roads linking townships like Diamond Creek, Kangaroo Ground and Hurstbridge.

A government township was reserved on the west side of Diamond Creek in the early 1850's. In 1863 the Diamond Reef was discovered on Dr Phipps' land east of the creek. This saw the establishment of a bustling township with a hotel, churches and school dating to the late 1860's and early 1870's.

After the gold rush Diamond Creek was cultivated for orchards and small farms. The town developed a more settled and rural character. By the time of the First World War the town could boast one hotel, a bank, police station, boot-maker's shop, Ryan's butcher shop, a Mechanics Hall and the Cool Stores. The population peaked around 1900 and then declined until the 1960's when new residents began to choose Diamond Creek as a place to live.

The rail line was extended to Diamond Creek by 1912 and further onwards to Hurstbridge to serve the local orchards. Orchards gradually declined facing competition from areas like Doncaster and Nunawading.

A number of houses around Collins Street are evidence of this early history as are the churches, Diamond Creek Primary School and a small number of commercial and community buildings. Bushfires have been part of the history of the town and claimed a number of important buildings.



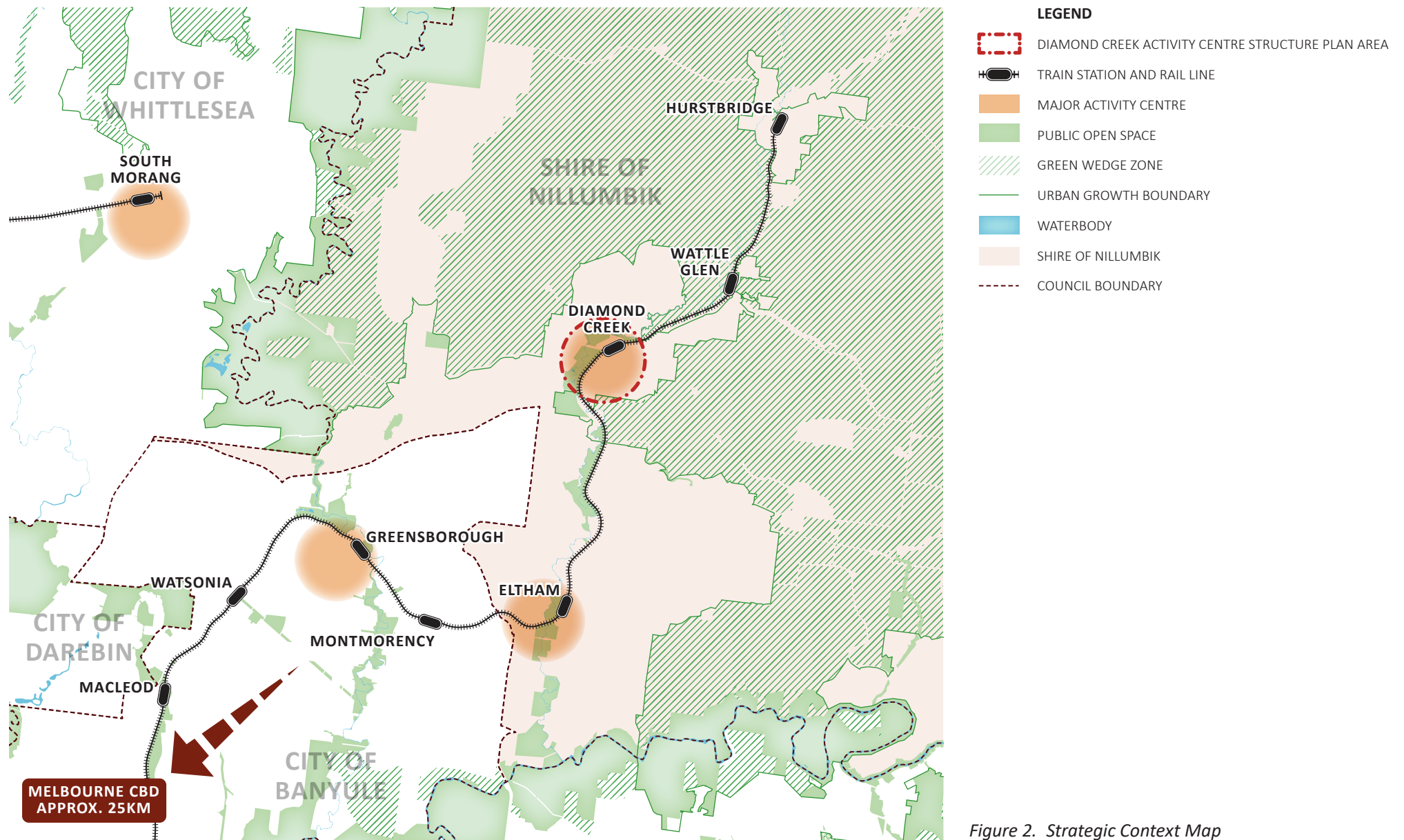


Figure 2. Strategic Context Map

## 2.3 What makes Diamond Creek Distinct?

Diamond Creek's unique style can be described as follows:

- Strong presence of trees and other vegetation.
- Rural and bushland character adding to the charm of Diamond Creek.
- Nestled within the Diamond Creek Valley.
- Architectural style and materials that complement the natural surroundings, vegetation and topography.

Significant elements of the Diamond Creek Activity Centre support the rural village character feel of the surrounding township. Key examples include historical buildings (dating back to the 1800s), natural landscape and native and exotic vegetation in its streetscapes, Diamond Creek Reserve, Nillumbik Park and hinterland beyond.

## 2.4 Key Influences

The main population, demographic and economic projections are based on the report *Nillumbik Activity Centres Land Use & Economic Analysis* (Geografia) and the 2016 Census Results.

The key influences, issues and projections can be summarised as follows:

### Fringe Location

- Low population growth in the Diamond Creek catchment as compared to Greater Melbourne.
- Less expenditure due to much of the Activity Centre catchment falling into Green Wedge zoned areas with very low density development.
- The importance of maintaining the activity centres attractive setting, therefore limiting redevelopment potential.

### Access and Movement

- High rates of car use (and poor public transport access).
- Increasing number of commuters travelling by car (192 persons increase from 2011 to 2016).
- Congestion due to the level crossing and road layouts not designed for high traffic volumes.
- Lack of pedestrian and cyclist amenity.

### The Catchment

- Population projections suggest an additional 28,909 residents in the Shire of Nillumbik by 2036.
- The Diamond Creek catchment has experienced modest population growth with a 4.6% increase from 2011 to 2016.
- High escape expenditure (that is economic activity from residents and local businesses leaking out of the Shire) due to much of the Activity Centre catchment falling into Green Wedge zoned areas with very low density development and much of the catchment areas overlapping with those of neighbouring activity centres.



- Only 57% of all spending in the Diamond Creek Major Activity Centre is by Shire residents, suggesting there is a significant, potential untapped revenue from the rest of the catchment.

### **Ageing population**

- Changing demographic profile from families with higher consumption patterns, to older couples (55+).
- Emerging older population (pre-retirees, retirees and seniors) and the need for smaller household (1-3 persons).
- Increased demand for health and other services.
- Changes in lifestyles, leisure and recreation.

### **Changing housing needs**

- Ageing, smaller households and changing needs such as less maintenance and easier access to services may change the needs or demand for housing.
- Need for a greater diversity in housing stock with the proportion of couples with no children increasing.
- Emerging number of medium and high-density housing (0.8% increase from 2011 to 2016).

### **Housing affordability**

- Diamond Creek's median housing price is \$805,600, keeping pace with Metropolitan Melbourne.
- Diamond Creek's median house rental is high as compared to Metropolitan Melbourne.

### **Employment**

- Employment in Diamond Creek grew by 19% from 2006 to 2016.
- Local jobs are predominantly in the retail sector.
- Diamond Creek is dominated by small businesses.

### **Land Supply**

- Lack of office space and local employment opportunities to suit the high number of professional workers living in the area.
- Commercial rents are reportedly high.
- A potential supply shortage of land and buildings coming onto the market.
- Decreasing number of manufacturing jobs, with increasing number of jobs in construction, health care and social assistance.
- Precincts of land (Precincts 3 and 5) utilised for industry not considered 'highest and best use'.

## 2.5 Community Engagement

ChatterBox Projects were engaged by Nillumbik Shire Council to undertake community engagement to inform the Draft Structure Plan.

Over 500 ideas were gathered throughout the community engagement process via the online and hardcopy surveys. Just over 150 additional opinions, thoughts and ideas were gathered via other engagement tools including the online mapping tool, voting pod, chatboards and big idea drawings.

Overall, respondents felt there was a good sense of community in Diamond Creek. However, they felt the atmosphere of the Town Centre was poor, it wasn't easy to get around and they raised issues around traffic and parking issues. However, the community loved the open space, trees and sporting facilities within the precinct.

The survey asked about people's connection to the Diamond Creek Activity Centre.

Of the 146 respondents:

- 81.5% live in or near Diamond Creek Activity Centre, 58.9% shop and 32.1% play sport within the precinct

- 53.9% visited Diamond Creek Activity Centre daily, 33.3% visited 2-3 times per week, and 6.4% weekly
- Most people visit the Activity Centre to shop (78.7%), visit cafes and restaurants (61.6%) for recreation (61.6%) and to use services (49.3%)
- Most people visit on weekday afternoons (68%), weekend mornings (65%), weekday mornings (59%) and weekend afternoons (59%). There were a lot fewer people visiting the activity centre during the evening (29%)
- 72.5% drive to the Activity Centre, 18.8% walk, 6.5% catch public transport and 1.4% ride

The community were asked to rate their level of agreement with statements about the Diamond Creek Activity Centre. The collated responses from the survey and pop-up consultations are given below:

- 68% agreed that the Activity Centre had a good range of quality shops and services available
- 76% agreed that the Activity Centre has a great sense of community
- 56% agreed that the Activity Centre has a great atmosphere and environment

- 46% said it was easy to get to and get around the Activity Centre
- 42% like the look and feel of the Activity Centre
- 99.3% feel safe during the day and 17.1% feel unsafe at night in the Activity Centre

Of the 104 respondents who shared their 'hidden gem,' 42.4% said the natural environment, trails and tracks, 27.3% said quality retail and dining options, 12.2% said leisure and sporting places and 10.3% said parks and playgrounds.

When asked whether they knew of any areas in the Activity Centre needing attention, the most frequently mentioned locations included the train station/railway crossing, areas around shopping centres, various recreation and leisure spaces, and specific streets. Concerns were mainly around traffic safety and flow, parking, and the appearance of buildings and public spaces.



## 3.0 An Overview of the Planning Framework

The following describes the planning framework of the Draft Structure Plan. The relevant content is summarised as follows.

### 3.1 Plan Melbourne Refresh 2017-2050

Plan Melbourne is a long-term plan to accommodate Melbourne's future growth in population and employment. It sets the vision of Melbourne as a global city of opportunity and choices, guided by a set of principles, directions and policies.

Within the Plan, Diamond Creek is identified as a Major Activity Centre. Relevant principles for the Draft Structure Plan aim to maintain the distinct character through complementary future developments, and to improve accessibility and movement for walking, cycling and public transport to encourage the creation of 20-minute neighbourhoods. Other relevant directions and policies focus on housing intensification in existing neighbourhoods, and pedestrian-friendly neighbourhoods.

### 3.2 Planning Policy Framework (PPF)

The Planning Policy Framework (PPF), and in particular Clause 11.03-1R, aims to create highly accessible activity centres with diverse employment uses supported by higher density housing.

It refers to the importance of building up activity centres as a focus of high quality development, activity and living for the whole community by developing a network of activity centres. In particular it discusses undertaking strategic planning for the use and development of land in, and around activity centres, focusing on increasing housing support and diversity, improving access by walking cycling and public transport and encouraging economic activity and business synergies.

Other relevant policies include:

- Clause 15.01-1S which seeks *“to create urban environments that are safe, healthy, functional and enjoyable and that contribute to a sense of place and cultural identity”*;
- Clause 16.01-1S which seeks *“to promote a housing market that meets community needs”*;
- Clause 17.01-1S which seeks *“to strengthen and diversify the economy”*;
- Clause 17.01-1S - which seeks *“to ensure the availability of industrial land”*;
- Clause 18.01-1S which seeks *“to integrate land use and transport planning”*; and
- Clauses 13.02-1S and 13.03-1S which seek to protect the resilience of existing properties and communities to bushfire and flooding risks.

### 3.3 Local Planning Policy Framework (LPPF)

The MSS outlines a vision that conserves the natural environment, historical buildings and sites, while allow for development within designated areas. Future land use and development will enhance the urban and rural environment, particularly the character defined by land form, landscapes and vegetation cover. This is reflected in the municipality's goals, which aims to encourage sustainable development, and retain the semi-rural character, natural and cultural heritage.

Clause 21.05-4 promotes both Diamond Creek and Eltham Major Activity Centres as the main commercial and community activity areas within the Shire. It seeks to concentrate the Shire's retail activities in the centres, ensuring future development is at a scale that responds to the scale of the centre while enhancing the natural and built form.

Similarly, Clause 22.14 'Diamond Creek Activity Centre Policy' encourages the expansion of commercial and community services, and increases to the diversity and amount of housing within the activity centre that is appropriately scaled to respect the surrounding topography.

### 3.4 Zones

The study area is predominantly within the Activity Centre Zone 2 (ACZ2) which seeks to accommodate a mix of land uses including additional higher density housing, whilst retaining the semi-rural character. The ACZ2 was introduced via Amendment C106 (October 2016) and references the following documents:

- *Diamond Creek twenty20 Major Activity Centre Structure Plan and Leisure Facilities Plan (September 2006)*
- *Nillumbik Major Activity Centres Sustainable Transport Study and Strategy, 2010*
- *Diamond Creek Major Activity Centre Car Parking Strategy, 2013*
- *Diamond Creek Major Activity Centre Design Guidelines Part A and Part B, 2014.*

Other zones within the study area include:

- Road Zone Category 1 (RDZ1);
- Public Use Zone 2 'Education' (PUZ2) and 4 'Transport' (PUZ4);
- Public Park and Recreation Zone (PPRZ); and
- Public Conservation and Resource Zone (PCRZ).

### 3.5 Overlays

The study area is affected by various overlays. Site specific Heritage Overlays (HO) apply to the Royal Mail Hotel (HO115), the Former Ryans Butcher Shop and Residence (HO133), the Former Milthorpe Store (HO259), the Diamond Creek Bridge (HO27) and Tree Rows (HO168). The Significant Landscape Overlay (SLO) Schedule 6 'Diamond Creek Major Activity Centre' applies highlighting the need to retain significant canopy trees that form part of the distinctive Diamond Creek character. Notably, a permit is required to remove, destroy or lop any substantial tree.

A Public Acquisition Overlay (PAO1) applies along the northern side of Main Hurstbridge Road for future road widening. A Parking Overlay (PO) also applies to commercial uses within the study area.

A Land Subject to Inundation Overlay (LSIO) applies which identifies land in a flood storage or flood fringe area affected by the 1 in 100 year flood or any other area determined by the floodplain management authority.

The LSIO predominantly impacts open space, recreation and community facilities in the Diamond Creek Reserve with minor encroachments into Precincts 1, 3 and 5. Flood risk and design constraints will need to be assessed by the



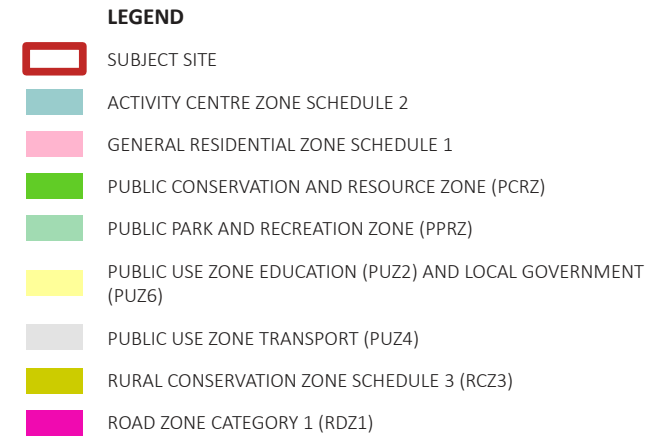
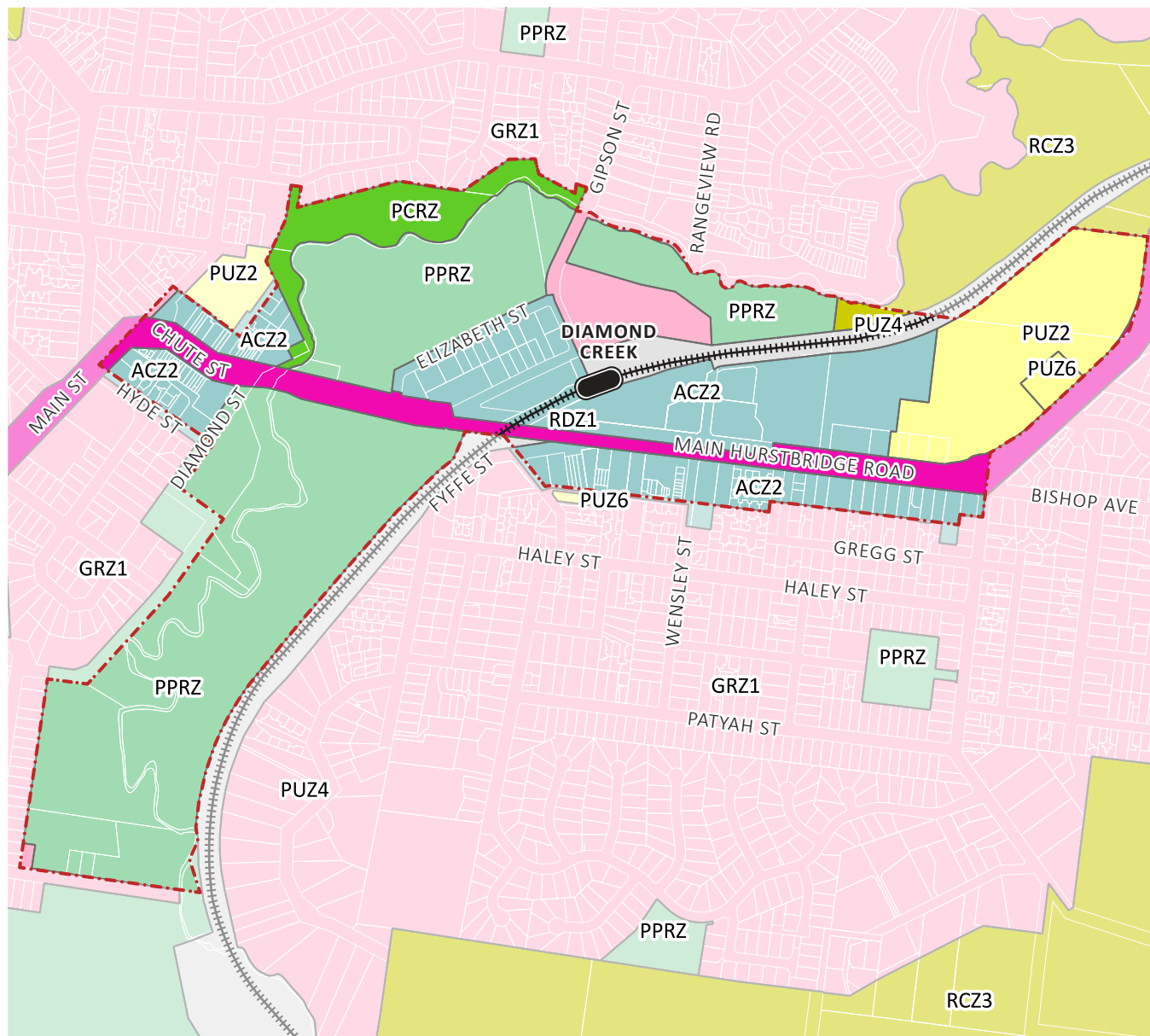


Figure 3. Zones

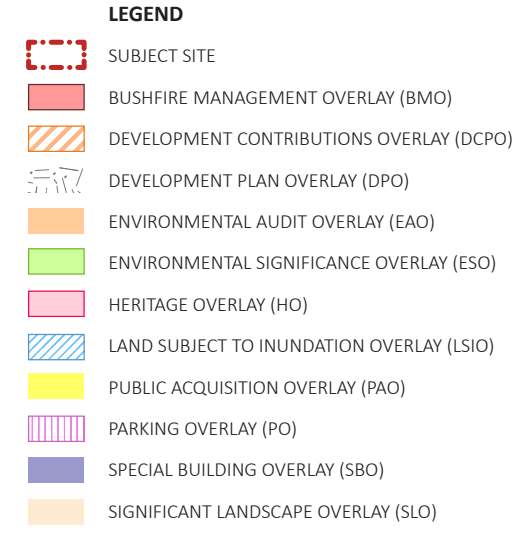
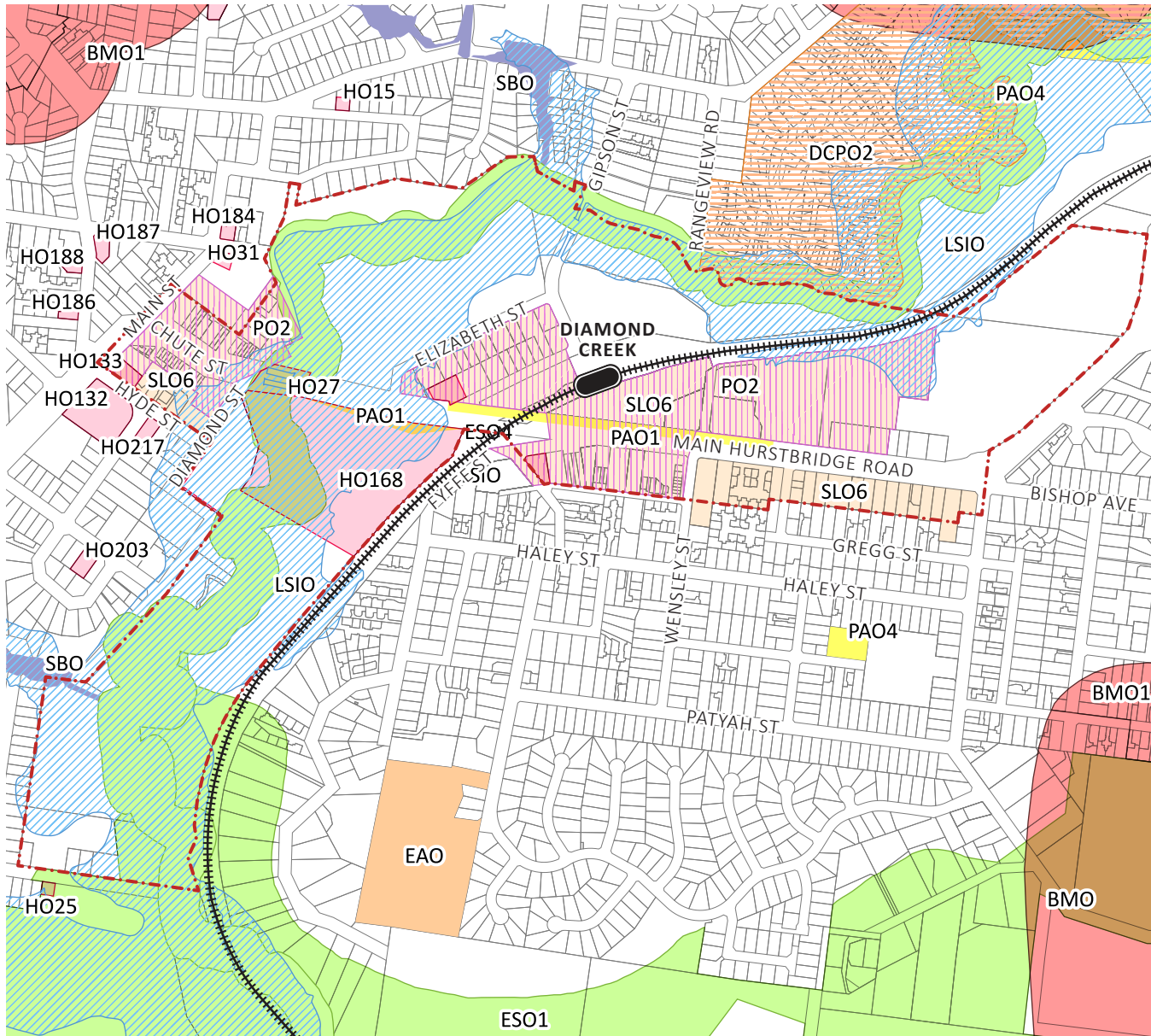


Figure 4. Overlays

## 4.0 The Vision & Structure Plan Framework

This section takes the reader from the Vision to Objectives and Strategies grouped under five themes underpinning this Draft Structure Plan. This is a four-step process as identified below.

1. **Vision** – What Diamond Creek Major Activity Centre will be in 10 years.
2. **Theme** – Planning, transport, economics and urban design themes as they relate to this Draft Structure Plan.
3. **Objective** – Statements about what we want to achieve.
4. **Strategies** – Ways of achieving the objectives.

### 4.1 Vision

The Vision statement underpinning the Draft Structure Plan is as follows:

*The Diamond Creek Major Activity Centre is envisioned as a mixed-use area with a public plaza at its heart, a place where the community can meet, transact business, recreate, learn, play, shop and work.*

*High quality, sustainable and innovative medium density developments will pay homage to its landscape setting and history.*

*Development will respond to and reflect the rural setting of the Diamond Creek Setting, particularly by their being a strong presence of native canopy trees.*

*Diamond Creek will not only build on its diverse array of employment opportunities, it will build its reputation as a boutique and artisanal food production destination.*

*It will be known for its distinctive rural village charm, environmental qualities and safe and walkable street network.*

### 4.2 Themes Objectives and Strategies

The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under the five themes of the Draft Structure Plan, which are explained in the following sections.

When brought together they provide the key ingredients of a uniquely semi-rural and economically viable activity centre on the fringe of metropolitan Melbourne.



## 4.3 Land Use Activities and Development

### Discussion

The Activity Centre experiences high escape expenditure, with unrealised night time or weekend economic capacity. This combined with the low population growth and functional inefficiencies including challenging terrain, low density, traffic congestion, poor pedestrian amenity and low public transport use, further exacerbate the continued loss of patronage to larger centres.

To create energy and vibrancy in the Activity Centre, changes to the existing land use pattern are proposed. This will not only enable the centre to better service local residents and support local businesses, it will also attract ongoing investment to the centre. The following sections provide an explanation of the key land use changes forecast for the activity centre.

### **Retail**

Future retailing opportunities within the Activity Centre are expected to relate to artisanal food production and hospitality, capitalising on the high amenity setting of Diamond Creek and the affluent residential population.

Further provision of cafes and restaurants would be highly complementary to residential development and the age and socio-economic profile of the local population. By introducing more lively activities in the Activity Centre, Diamond Creek will gain a more safe and vibrant environment both day and night.

Precinct 4 to the south of the rail line is expected to continue its role as a retail location in its current form. With the intensification of sites within the activity centre, there will be new opportunities for office space at ground or first floors of mixed-use development.

### **Office and Mixed Use**

The Activity Centre currently lacks office floorspace or a large employment anchor. Employment is predominantly in retail with industrial businesses in Precincts 3 and 5. The Activity Centre requires a generator of employment and investment.

The land use and economic analysis completed by Geografia suggests a key strategy may be to investigate the potential rezoning of Precincts 3 and 5 to a Commercial 3 Zone (C3Z), which would provide for a range of industrial, commercial, office and other employment generating uses which support the mixed-use function of the area.

Mixed use in Precincts 3 and 5 would breathe life into the centre, allowing small businesses and creative industries to emerge and for Diamond Creek to become a hub of innovation. The C3Z will also ensure that the use mix can be tailored to ensure it complements Precinct 4, rather than competing with it. It also enables the retention of the industrial uses in the precincts in the interim, which are an employment driver in the activity centre.

Rezoning of Precinct 5 C3Z also creates the opportunity for investigation into a potential health or civic anchor. Moving the Shire Offices to Diamond Creek for example, would create a much needed major employment anchor, increase the viability of the shops and services, and bring vitality to the centre.

To achieve the mixed use outcomes explained in the previous paragraphs, Council could also modify the existing ACZ2 which would have a similar effect to the application of a C3Z, while not necessitating a rezoning.

## Housing

More dwellings in and around the Activity Centre will support a lively and functioning centre. The Activity Centre has seen almost no residential densification due in part to a lack of available land, limited demand for housing, difficult topography, sensitive landscapes and community opposition.

A strategy is included that encourages redevelopment at increased residential densities to achieve an increase of 303 more dwellings within walking distance of the town centre by 2036.

Residential development should be predominantly located in Precincts 4 and 6. The potential use of a C3Z (or modified ACZ2) in Precincts 3 and 5 enables Council the ability to restrict residential development to a percentage of the total floorspace.

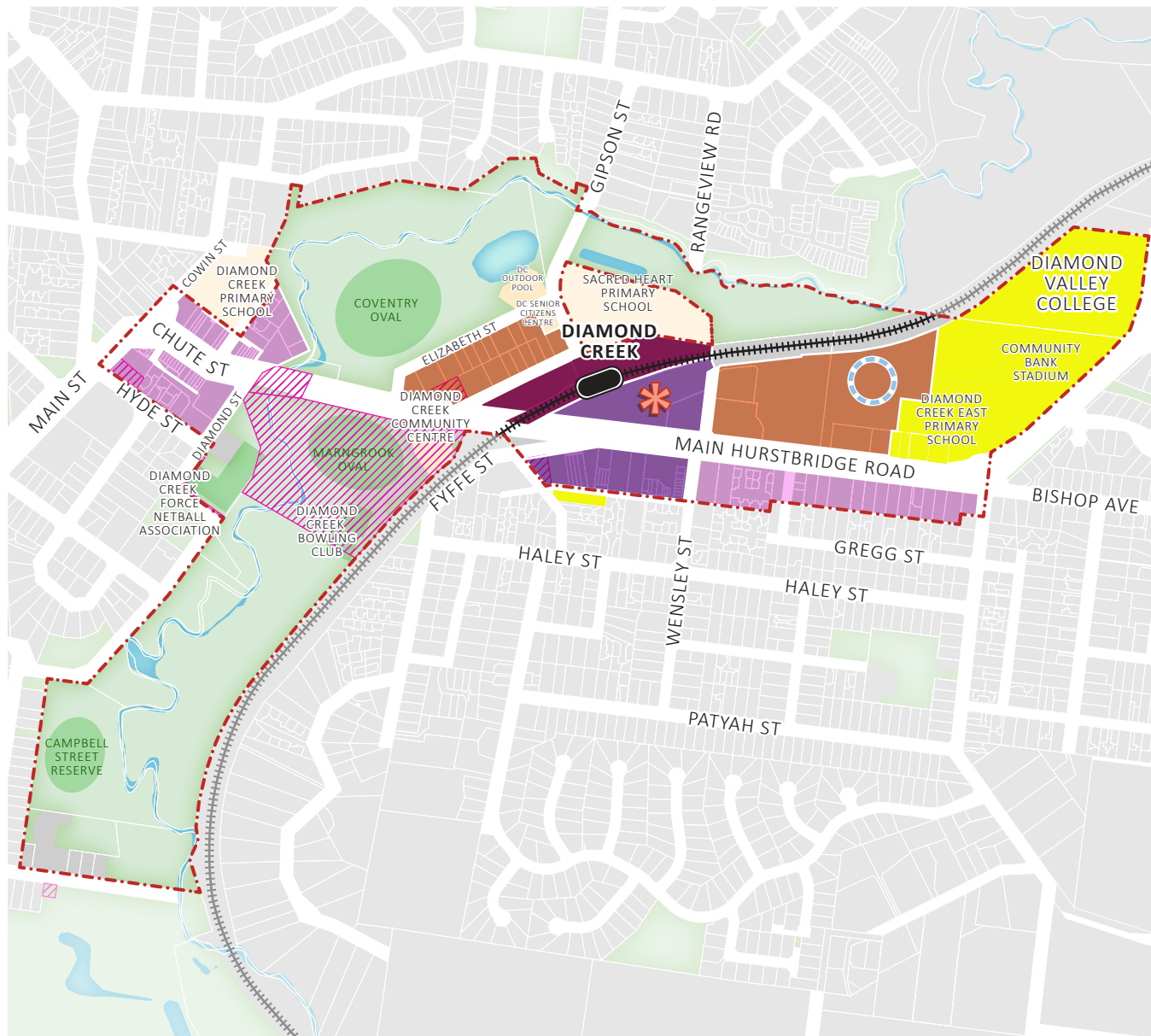
## Objectives

- To expand employment floor-space, particularly emergent industries, to meet an increased proportion of projected demand and reduce 'escape expenditure' and travel to nearby centres.
  - To support existing industrial uses whilst facilitating other compatible emergent industries to energise the centre.
  - To enable an employment anchor in the Diamond Creek Major Activity Centre, such as a civic or health anchor.
  - To develop additional higher density housing in the centre to meet projected needs and increase dwelling type diversity and energise the centre.
  - To provide accessible and affordable housing options close to public transport.
- To focus retail activity in a compact core east of the railway, on the 'landscaped civic spine' of Main Hurstbridge Road, with a retail node in Chute Street.
  - To cluster community and leisure facilities in and around the centre as added attractions for the community, to meet local needs and complement retail and commercial activity.
  - To improve access to and integrate retail, commercial and housing development with the railway station and its surrounds.

## Strategies

- Support and further develop Diamond Creek's reputation as a boutique and artisanal food production destination.
- Develop land use provisions that support a diversity of employment opportunities.
- Encourage uses that extend the hours of activity of the activity centre, develop an evening economy and strengthen its function as a place for living and working at the street level.
- Provide the opportunity for a transit-oriented development (TOD) on land to the west of the railway line (Precinct 4) including uses such as commercial and residential.
- Undertake community consultation to review the potential application of C3Z (or a modified ACZ2) in the existing light industrial Precincts 3 and 5 to allow for a diversification of uses from creative industries, small manufactures, start-up businesses to offices, to facilitate business growth and employment opportunities.
- Encourage redevelopment at increased residential densities to provide additional dwellings within walking distance of the town centre.
- Deliver a vertical mix of land uses through facilitating the use of shop tops for office, residential and other commercial uses within the main streets of the Activity Centre.
- Investigate the potential sites for an employment anchor, such as a civic or health facility in the Activity Centre, potentially in Precinct 5.





## LEGEND

- STUDY AREA
- TRAIN STATION AND RAIL LINE
- EXISTING PUBLIC OPEN SPACE AND OVALS
- WATERBODY
- EXISTING PUBLIC USE ZONE
- EXISTING HERITAGE OVERLAY
- EXISTING COMMUNITY/ EDUCATION FACILITIES
- OPPORTUNITY FOR A TRANSIT ORIENTED DEVELOPMENT (TOD) CONSISTING OF COMMERCIAL WITH RESIDENTIAL ABOVE
- DIVERSIFY EXISTING RETAIL WITH EMERGENT INDUSTRIES (BOUTIQUE, ARTISANAL FOOD PRODUCTION, CAFES, ETC.) WITH OFFICE AND RESIDENTIAL AT UPPER LEVELS
- POTENTIAL APPLICATION OF MODIFIED ACZ TO FACILITATE CREATIVE INDUSTRIES SMALL MANUFACTURERS/ START-UP BUSINESSES AND SOME RESIDENTIAL
- OPPORTUNITY FOR MIXED USE INCLUDING RESIDENTIAL, OFFICE, MEDICAL
- OPPORTUNITY FOR CIVIC OR HEALTH-RELATED EMPLOYMENT ANCHOR
- EXISTING RETAIL ANCHOR

Figure 5. Land Use Activities and Development Framework Plan

## 4.4 Built Form Character

### Discussion

The Study Area is characterised by clusters of built form at 1-2 storeys in scale spread across the Activity Centre. It has a distinct lush landscape setting nestled into Diamond Creek Valley with native trees greening the commercial spine.

Taking into consideration the scenic and environmental qualities and rural township character, it is anticipated future growth is relatively modest compared to other major activity centres that are located closer to the CBD.

It is most likely that the forecast for moderate residential growth can be accommodated through infill development designed to respond to the existing character.

The Draft Structure Plan proposes built form of varied scales that generally align with the urban design provisions within the ACZ2. Only minor changes are proposed to future building heights and setbacks which include:

- Increased application of 5 storeys adjacent to the train station in Precinct 4 to capitalise on the high level of access to public transport and lack of

sensitive interfaces;

- Increased application of 5 storeys in Precinct 3 due to the lack of sensitive interfaces; and
- Increased application of 4 storeys in Precinct 1 to capitalise on creek frontage.

The tallest buildings (5 storeys) are clustered around the train station responding to the high accessibility to public transport.

5 storeys is also proposed in Precinct 3, directly adjacent to the train station and away from sensitive residential interfaces. Future development in Precinct 3 will need to respond to the heritage features of the Diamond Creek Tavern.

The built form character along Main Hurstbridge Road is varied. Commercial buildings on the southern side of Main Hurstbridge Road (generally between Brooke Crescent and Wensley Street) express a finer grain form than what is found on the northern side.

On balance however, extensive front setbacks for car parking dominate the streetscape and create a lack on built form enclosure further emphasised by large road verges. Shopping complexes that are setback from Main-Hurstbridge Road due to car parking in the frontage create

a poor pedestrian environment and a lack of activation.

New built form along Main Hurstbridge Road in Precinct 4 should have a 0-metre setback and provide a consistent street wall, with a setback only to accommodate existing landscaping in the public realm.

Street wall heights should be designed to respect and respond to the existing built form, creating a continuous built form scale with recessive upper levels. Street walls should be no greater than 2 storeys, with upper levels (above the 2nd storey) recessed by at least 3 metres.

Built form in Chute Street is predominantly single storey, fine grain in style (closely spaced, narrow buildings) and built to express a continuous street wall to the public realm. Future built form should respond to the grain, scale and style of the existing character.

Precinct 6 consist of a mix of commercial and residential uses within detached buildings (originally dwellings) with generous side and rear setbacks and 1-2 storeys in height. Future development will need to respond to the existing character with front, side and rear setbacks.

The Built Form Character Framework Plan depicts maximum building heights within each precinct and articulates where an individual design approach is required.

Maintaining reasonable amenity for residential properties adjoining and adjacent to the Activity Centre is also a key consideration. Taller development can adversely affect the amenity of adjacent residential properties through visual bulk, overlooking and overshadowing.

Such impacts are to be reduced by setting back upper levels at the residential interface. Refer to the General Guidelines at Section 6.0 for interface setback requirements.

### **Objectives**

- To retain the semi-rural township character of Diamond Creek by clustering new development and activity in existing key locations that remain visually segregated by open spaces and vegetation.
- To develop a local Diamond Creek architectural style that complements and reflects its natural landscape setting, and ensure new or redeveloped buildings are of high architectural quality appropriate to the character of the centre.
- To design new buildings with well-articulated facades and a fine grain to provide interest at the street level and reinforce the human scale of shopping areas.
- To ensure development retains and enhances the township's character with medium scale buildings, that are respectful to existing buildings, materials that complement the setting and a treed landscape of predominantly local indigenous canopy trees.
- To maintain appropriate levels of solar access to existing and proposed public spaces.
- To protect and enhance the amenity within the Activity Centre and surrounding established residential neighbourhoods.
- To avoid underdevelopment of land within the activity centre.
- To promote sustainable development including water sensitive urban design.
- To establish energy efficient developments that are appropriate to local conditions
- To ensure new development manages off-site amenity.
- To increase activity and passive surveillance in the main retail and mixed-use areas.
- To reduce the visual dominance of car parking and ensure the design of parking and access areas is safe, practical and attractive.
- To ensure that pedestrian routes, streets, footpaths and open spaces interact with and are overlooked by buildings, and for these routes to be continuously accessible.



## Strategies

- Building heights across the activity centre should be no greater than 5 storeys, with 3 storeys adjacent to existing residential hinterland.
- Tallest buildings (5 storeys) are clustered around the train station and within the Elizabeth Precinct, away from sensitive low-scale residential interfaces.
- Redevelopment along Main Hurstbridge Road should be built to the front boundary with car parking either provided in basements or behind the building.
- The street wall height should respond to the existing built form, creating a continuous built form scale with recessive upper levels.
- Heritage fabric should be retained. Upper levels must be setback behind the street wall in line with the heritage fabric retention.
- Redevelopment should respond to the architectural style and materials that complement the natural surroundings, vegetation and topography.
- Redevelopment should respond to the distinct fine grain character in Chute Street and the southern side of Main Hurstbridge Road.
- Redevelopment should respond to the rural village character.
- Buildings should be designed to respond to the slope.
- Buildings front, side and rear setbacks should be responsive to the adjacent character and landscaping.
- Future redevelopment of the Diamond Creek Tavern should respect its heritage features. Built form adjacent should ensure it does not dominate the heritage form.
- Active frontages should be provided along Station Street, Elizabeth Street, Chute Street and Main Hurstbridge Road.

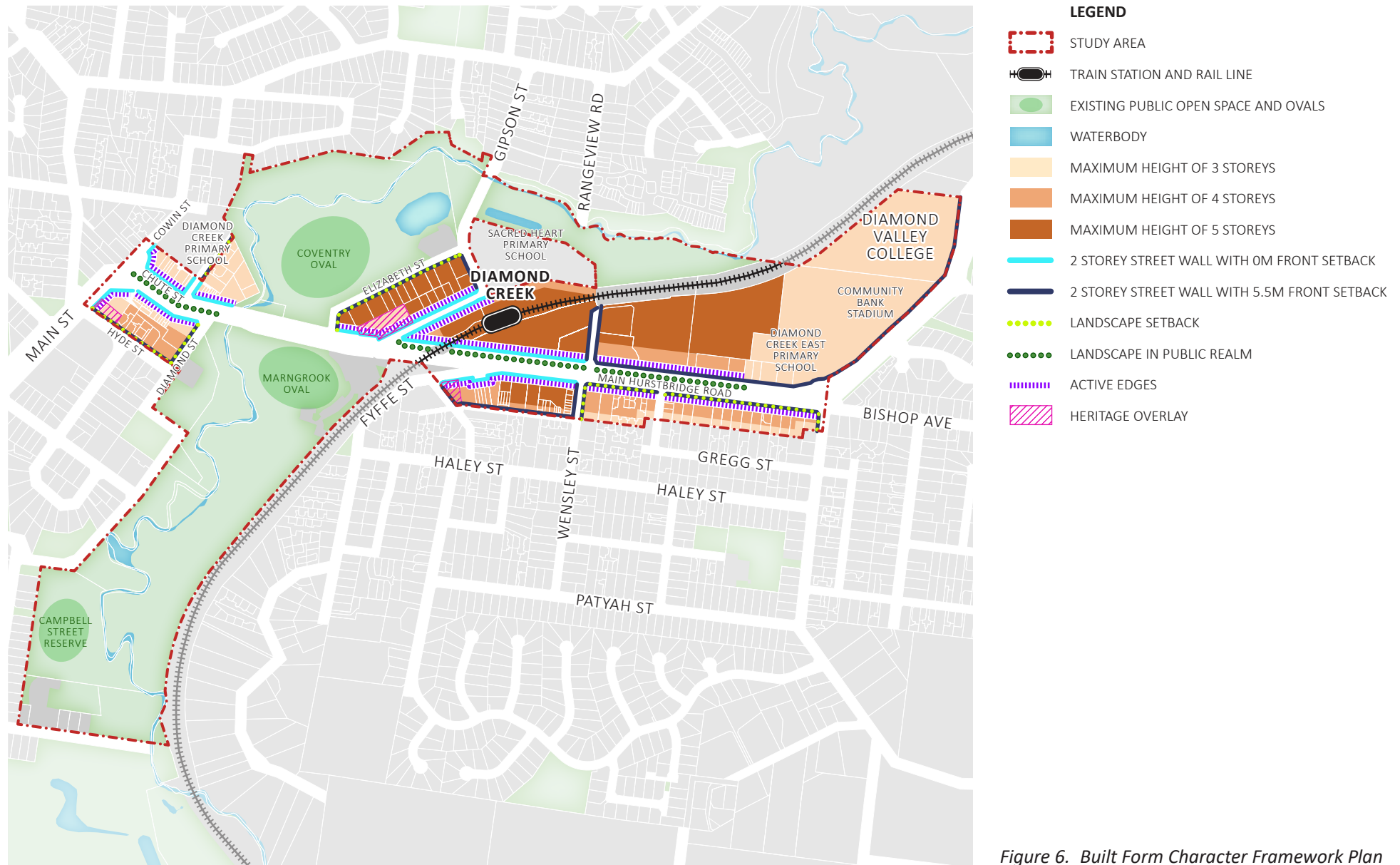


Figure 6. Built Form Character Framework Plan

## 4.5 Movement and Access

### Discussion

Diamond Creek is poorly serviced by public transport, with train services operating infrequently (1 every hour at peak times) from Diamond Creek Train Station. Bus services are provided along Main Hurstbridge Road, Station Street and Gipson Street.

Diamond Creek has low public transport patronage levels with residents driving to the centre. This creates high levels of congestion in the activity centre, further exacerbated by the existing level crossing.

In summary, the key access and movement issues are:

- Poor pedestrian amenity, particularly along Main Hurstbridge Road due to disconnected and narrow footpaths and limited signalised pedestrian crossing points;
- Poor pedestrian safety and connectivity issues due to the rail line splitting the activity centre in half;
- Poor connectivity of the existing Diamond Creek trail across Main Hurstbridge Road;
- Lack of cycling infrastructure including on-road cycle lanes and bike parking facilities;

- Poor pedestrian access and traffic flow at the intersection of Main Hurstbridge Road and Fyffe Street leading to major congestion and safety issues, particularly when the level crossing is down; and
- Traffic congestion due to the level crossing and high car usage levels.

State planned infrastructure will also have on flow impacts for the activity centre. Duplication of the Hurstbridge line is proposed, which will increase the frequency of train services. However, the increased frequency will also increase traffic congestion due to the existing level crossing.

Removal of the level crossing is critical to address severe congestion, connectivity and safety concerns. The Draft Structure Plan includes strategies that advocate for removal of the level crossing. The removal of the level crossing could be achieved by either going underground or the use of a sky rail. However, given the increased technical implications relating to the potential flooding from the creek, a Sky Rail Concept Plan, which identifies the indicative location of a future elevated rail line and new station has been developed at this time for discussion purposes.

Elevating the rail line would not only alleviate traffic

congestion, it would also connect the northern and southern sides of the rail line, Activity Centre and create ample opportunities for public open space under the rail line. A sky rail concept plan is provided at Figure 8. It is supported by examples of potential types of linear public open space that could be provided under the rail line.

As the Diamond Creek level crossing removal is not scheduled to be removed as part of the State Government's removal works, the Draft Structure Plan includes strategies regarding the improvement and implementation of additional pedestrian/ cyclist bridge over the rail line to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond.

In relation to the provision of car parking within Diamond Creek, generally there is capacity except for the commuter car park. However, alternative spaces are available in adjacent public car parks. Based on the current growth projections, additional land for car parking is not considered necessary. However, a re-design or re-optimisation of the existing commuter car park would create increased capacity, improve safety as well as internal car parking circulation

Main Hurstbridge Road has a multi-functional role as the



main activity spine for the activity centre and the main gateway to the Green Wedge part of the Shire, and some of the State's most bushfire vulnerable land.

The Draft Structure Plan must strike a balance between maintaining traffic low, avoiding congestion, whilst creating a pedestrian friendly activity centre, and better connectivity with the northern and southern sides of Main Hurstbridge Road.

Overall, the Draft Structure Plan provides the framework to advocate for key infrastructure items, while also directing upgrades to footpaths and cycle networks to improve pedestrian and cyclist amenity within the activity centre.

### **Objectives**

- To optimise the potential to reduce car dependency within the community by developing a network of local and regional shared paths for recreational and functional use, and by encouraging upgrades to the Hurstbridge line to increase the frequency of public transport services.
- To advocate for removal of the level crossing to create better pedestrian and cyclist connectivity north-south through the centre.
- To improve car park management and design.
- To establish the commercial and retail precincts of the township as 'pedestrian priority areas'.
- To improve access to the centre for all modes of travel, link the precincts of the town physically with clear pedestrian/cycle paths and visually with landscape treatments.
- To expand the range of public transport linking the centre and the role of the railway station as a multi-modal interchange.
- To create better pedestrian and cyclist connectivity across the railway line.
- To encourage traffic calming measures to Main Hurstbridge Road to improve conditions for pedestrians and cyclists.
- Improve pedestrian connectivity, safety and amenity through existing at grade car parks, and across the railway line.

## Strategies

- Investigate improvements to the intersection of Main Hurstbridge Road and Fyffe Street to reduce congestion and alleviate safety concerns.
- Liaise with the Department of Transport (DoT) to advocate for the removal of the level crossing and potential implementation of a sky rail as per the concept plan provided at Figure 8.
- Investigate the opportunity to make Station Street one way, to lessen conflicts between cars parking and pedestrians and reducing vehicular turning movements, minimising congestion.
- Investigate implementation of indented parking on the western side of Elizabeth Street.
- Redesign or re-optimize existing commuter car park to improve safety as well as internal car park circulation.
- Liaise with DoT to advocate for the implementation of on-road cycle lanes on Main Hurstbridge Road.
- Liaise with Vic Track and DoT to investigate additional pedestrian/ cyclist bridge over rail to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond.
- Liaise with the DoT to improve existing bus stops along Main Hurstbridge Road.
- Investigate the opportunity to provide a raised pedestrian crossing directly east of Fyffe Street, connecting the northern and southern sides of Main Hurstbridge Road.
- Create a clear cycling route from the Train Station to Chute Street and the Diamond Creek Trail.
- Implement proposed bike path connecting the Diamond Creek Trail to Hurstbridge to the north east.
- Implement lighting, paving, and landscaping through existing at grade car parks and across the railway line, ensuring they are safe and inviting spaces.
- Investigate the potential for an underpass connecting the Diamond Creek Trail under Main Hurstbridge Road, connecting the northern and southern open spaces.
- Provide bike and mobility scooter parking at highly frequented facilities such as supermarkets and the train station.
- Reduce the speed limit along Main Hurstbridge Road to 40km/h.
- Implement Gipson Street bridge duplication as specified in DCPO1, to improve traffic flow to the Activity Centre. As part of this, investigate complementary measures to:
  - Improve the amenity of the bridge as an entrance into the centre.
  - Enhance the connectivity the bridge provides for pedestrians/ cyclists in and out of the centre (e.g. clear provision for cyclists and pedestrians on the bridge).
  - Provide a safe and non-congested environment in the Station Street precinct, particularly by considering traffic calming devices, pedestrian crossings, carefully-placed parking, etc.

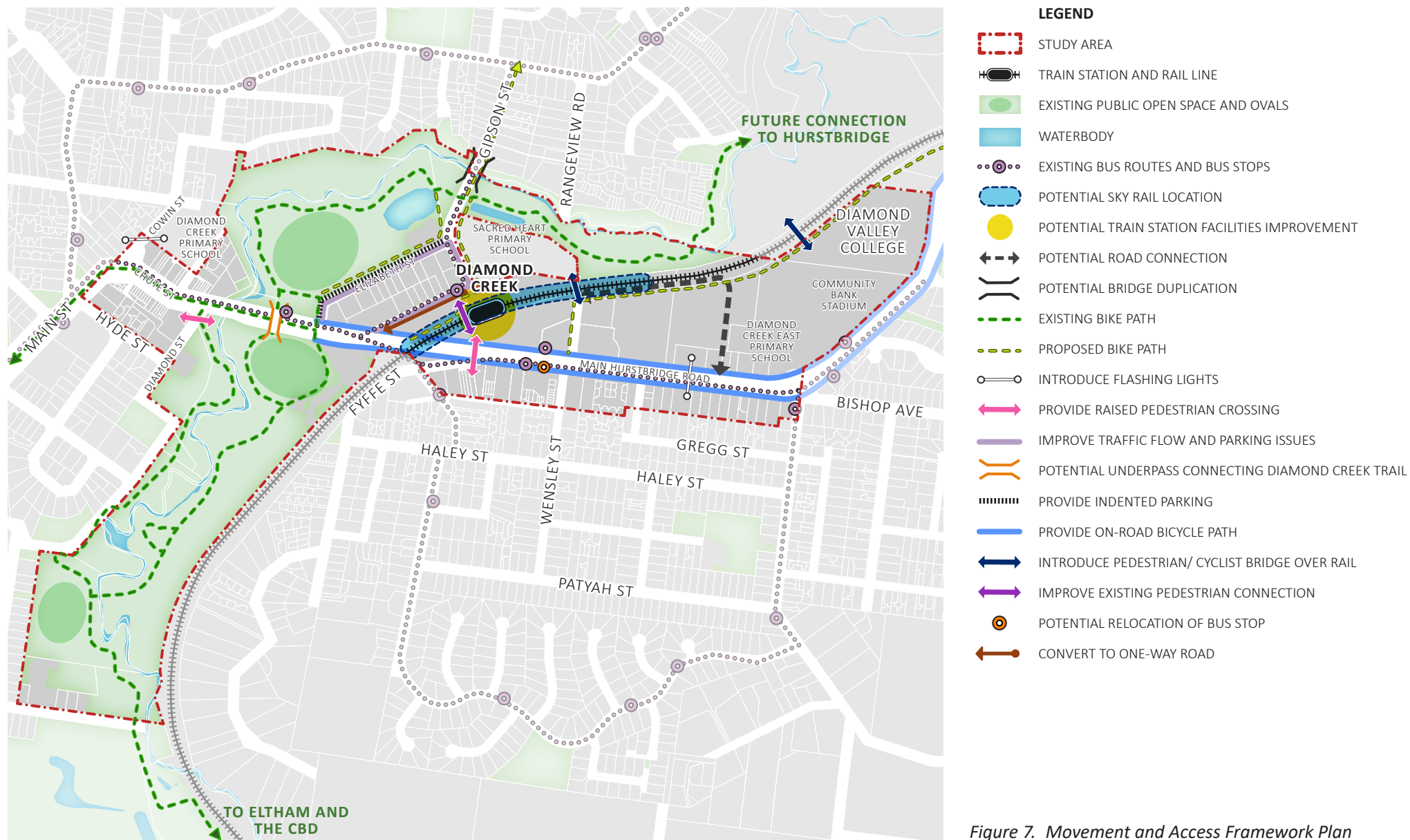


Figure 7. Movement and Access Framework Plan



## LEGEND

- POTENTIAL LOCATION/EXTENTS OF ELEVATED RAIL LINE
- POTENTIAL NEW STATION LOCATION
- POTENTIAL REDEVELOPMENT
- ACTIVE EDGES
- POTENTIAL FOR FORMALISED PEDESTRIAN/ BIKE CONNECTION
- PLAZA/ SHARED SURFACE
- TOWN CENTRE HEART
- OPPORTUNITY FOR PUBLIC OPEN SPACE BELOW SKY RAIL
- BIKE PATH

Figure 8. Diamond Creek Sky Rail Concept Plan





Figure 9. Precedent images of linear open spaces and train station (Source 1: <https://svc.com.au/project-items/carnegie-station-linear-park/>; Source 2: <https://landezine-award.com/caulfield-to-dandenong-level-crossing-removal-project/>)

## 4.10 Landscape and Place Activation

### Discussion

The Activity Centre is nestled in the Diamond Creek Valley, with its natural landscape and low-scale buildings creating a rural village character. A key concern for the community is retention of and provision of more landscaping in the Activity Centre.

The commercial components of the Activity Centre are predominantly characterised by 1 to 2 storey buildings with car parking within the front setback. This combined with congestion along Main Hurstbridge Road create a poor pedestrian environment.

Increased presence of vegetation in the public realm will help to improve the pedestrian experience, creating shade and climate control, whilst also blending the built form into the landscape. Other streets that would benefit from landscape improvements include Station Street, Elizabeth Street and Chute Street.

Parts of Main Hurstbridge Road, Chute Street and Elizabeth Street lack public realm amenity, with limited signalised pedestrian crossing points, cycle lanes and footpaths. Key improvements particularly along Main Hurstbridge Road include continuous and widened footpaths.

The Activity Centre is also lacking a public space/ plaza where people can meet, rest and linger. A strategy of the Draft Structure Plan recommends investigating the opportunity for a public space/ plaza to be located adjacent to the train station north of Main Hurstbridge Road as shown at Figure 10.

The land between the rail line, Main Hurstbridge Road and George Street could also be redeveloped to include an internal street adjacent to the public space/ plaza, that creates a new 'heart' to the Activity Centre away from the traffic congestion on Main Hurstbridge Road.

Due to the multi-functional role of Main Hurstbridge Road and the size of the centre and its catchment, it is unlikely that both sides of the road can support the energy and vitality needed.

### Objectives

- To develop more attractive public spaces to enhance social interaction and the image of the centre.
- To incorporate references to European and indigenous heritage in the landscape and use art in public spaces.
- To retain and develop the open spaces within the centre and the links to parkland along the nearby creek floodplain as a key feature of the centre.
- To create a 'landscaped civic spine' along Main Hurstbridge Road as the connecting feature of Diamond Creek.
- To retain and increase vegetation in the public realm.
- To ensure that development sites with front, side or rear setbacks are extensively landscaped with indigenous vegetation.
- To maintain the visual dominance of the well vegetated natural landscape throughout the residential, commercial and open space areas of Diamond Creek.

## Strategies

- Strengthen the landscape character along Main Hurstbridge Road through the provision of mature trees and planting within the streetscape and public realm.
- Investigate the opportunity for a public space/ plaza and the 'heart' of the Activity Centre to be located adjacent to the train station north of Main Hurstbridge Road away from the traffic congestion.
- Prepare an Urban Design Framework (UDF) for Precinct 4 that incorporates a public space/ plaza and the 'heart' of the Activity Centre to the north of Main Hurstbridge Road away from traffic congestion.
- Enhance the public realm with seating, shade trees and space for outdoor eating.
- Implement footpaths along northern side of Station Street and Gipson Street, and between Station Street and the Coles Supermarket on Main Hurstbridge Road.
- Investigate the opportunity for a raised pedestrian crossing directly east of Fyffe Street, connecting the north and south sides of Main Hurstbridge Road.
- Implement improvements to the existing railway pedestrian crossing.
- Investigate the provision of additional pedestrian railway crossings to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond.
- Investigate potential footpath upgrades and widening along Main Hurstbridge Road.
- Investigate improved pedestrian infrastructure at the intersections of Cowin Street, Diamond Street and Main Hurstbridge Road.
- Development should respond to the distinct landscape character in Precinct 5 through maintaining front setbacks and landscaping. The front setback should be a minimum of 5.5m measured from the front property boundary.



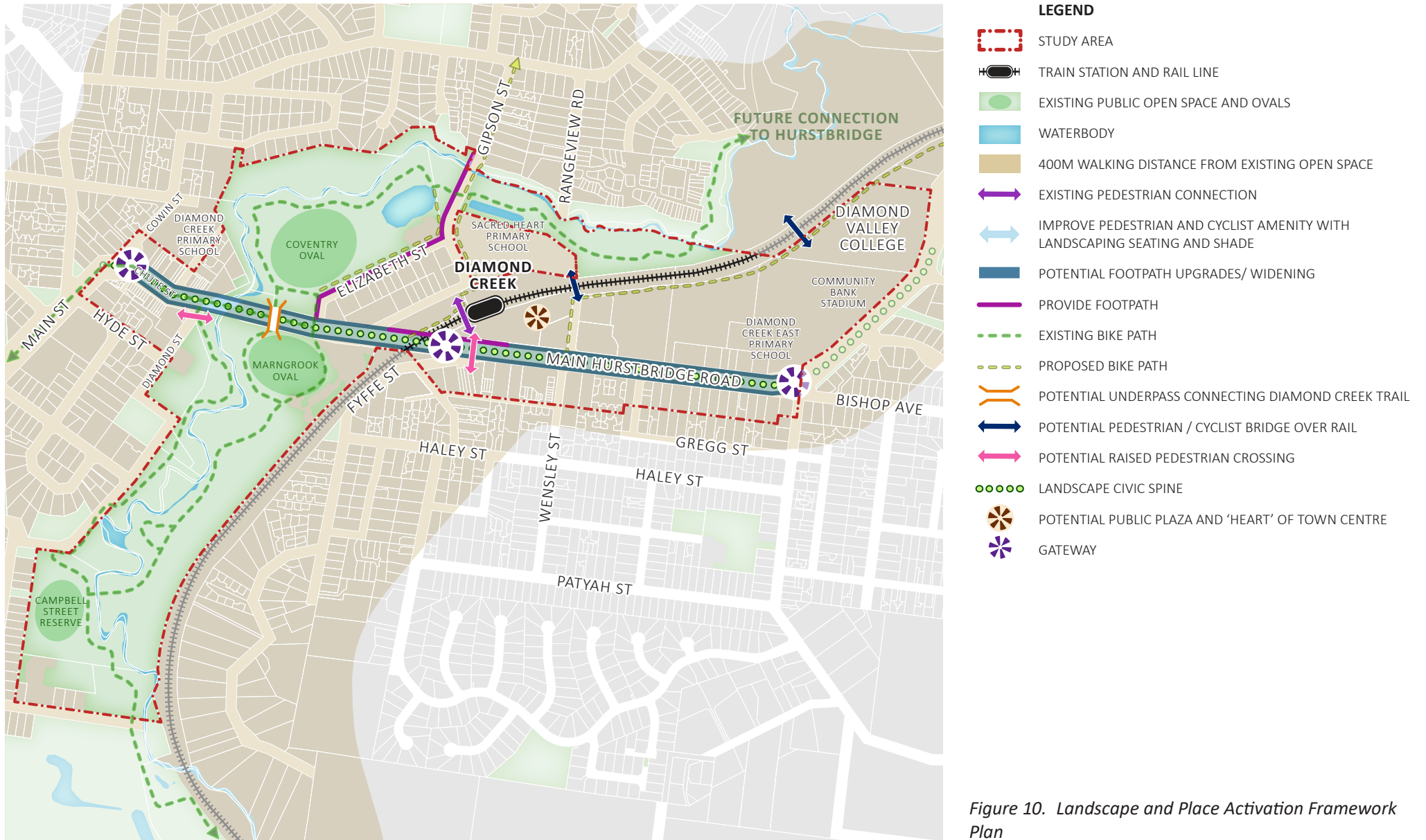


Figure 10. Landscape and Place Activation Framework Plan



## 4.11 Community and Leisure

### Discussion

Diamond Creek is characterised by a network of open space that creates a continuous green corridor through the centre. The creek corridor has naturally created space for an abundance of passive and active recreation activities and community facilities.

### **Passive and Active Recreation Facilities**

The Diamond Creek Trail is highly utilised by residents and connects to Eltham and through to the CBD. The community describes the trail as a hidden gem. An extension to the existing trail through to Hurstbridge is currently being constructed.

The Draft Structure Plan advocates for improvements to the existing trail infrastructure to improve user safety and experience, including more drinking taps and improved lighting.

Diamond Creek's sport facilities are not just used by the local community but serve a regional catchment of schools and sport clubs. In relation to recreation facilities such as sports ovals, pavilions, netball, tennis and bowls facilities, the provision levels are high for the population.

However, many of the existing facilities require updating as they are non-compliant. Consideration also needs to be given to Diamond Creek's aging population, with a greater number of older workers and pre-retirees than the greater Melbourne average (16% versus 12%). Improvements are already underway to upgrade aging infrastructure as well ensuring lifetime play is catered for.

Works recently completed or currently underway include:

- 3 fitness equipment stations recently installed;
- Marngrook Oval Pavilion Development currently under construction;
- Planned demolition and rebuild of Diamond Creek netball pavilion and upgrade of the car park.
- Proposed upgrades to lighting and Diamond Creek ovals;
- Proposed pavilion development for Coventry Oval; and
- Proposed synthetic soccer grounds at Diamond Creek Valley Soccer Club/ Diamond Creek East Primary School.

To cater for the regional catchment a new regional playspace is proposed. The Diamond Creek Regional Playspace is proposed to be situated on the old pony club

site, adjacent to Marngrook Oval, Diamond Creek Bowls Club and off-lead dog park. The playspace is proposed to be nature-themed and cater for all ages and disabilities. It will also include a refurbished W-class tram that will operate as a community café.

The Draft Structure Plan advocates for the above works to ensure the activity centre caters for the recreational needs of existing and future communities.

### **Community Facilities**

Existing community facilities have been assessed to be mostly aging, single-purpose and of varying quality and use. An obvious deficit in the Activity Centre is the lack of a multi-purpose community building. It is understood that the Diamond Creek Community is predominantly utilised as a fitness centre. Community meeting spaces are mainly well-used and offered by the Senior Citizens Building and Living and Learning Building, with a further meeting room at the Community Bank Stadium. However, these facilities are not co-located and unattractive. The forecast doubling of retirement-age residents necessitates the introduction of a disability compliant, multi-purpose facilities.

Based on the above, any community infrastructure will need to be planned and upgraded to meet the activity and inclusion needs of the older population. The need for a multi-purpose community building should be considered. The multi-purpose community building could also include youth friendly community space, community arts and performance space and a library. This could be provided for through redeveloping the Diamond Creek Community Centre.

## **Objectives**

- To provide for a network of spaces and facilities to meet projected community needs.
- To ensure community and leisure facilities are accessible to everyone, regardless of age or disability.
- To provide a multi-purpose community facility in the Activity Centre.
- To enable the location of a civic building (Council Offices or other) within the Activity Centre.

## **Strategies**

- Investigate the capacity of the existing Diamond Creek Community Centre to be refurbished to be a multi-purpose community facility that includes a variety of services and spaces for all age groups and potentially a library.
- An Accredited Access Consultant should review upgrades required to sports ovals, pavilions, netball, tennis and bowls facilities to ensure they are accessible to all.
- Advocate for the implementation of the Diamond Creek Regional Play Space and ensure it encompasses facilities for lifetime play.
- Advocate for improvements to the Diamond Creek Train to improve user safety and experience, including more drinking taps and improved lighting.
- Investigate opportunities to create additional meeting spaces adjacent to Coventry Oval and Main Hurstbridge Road.

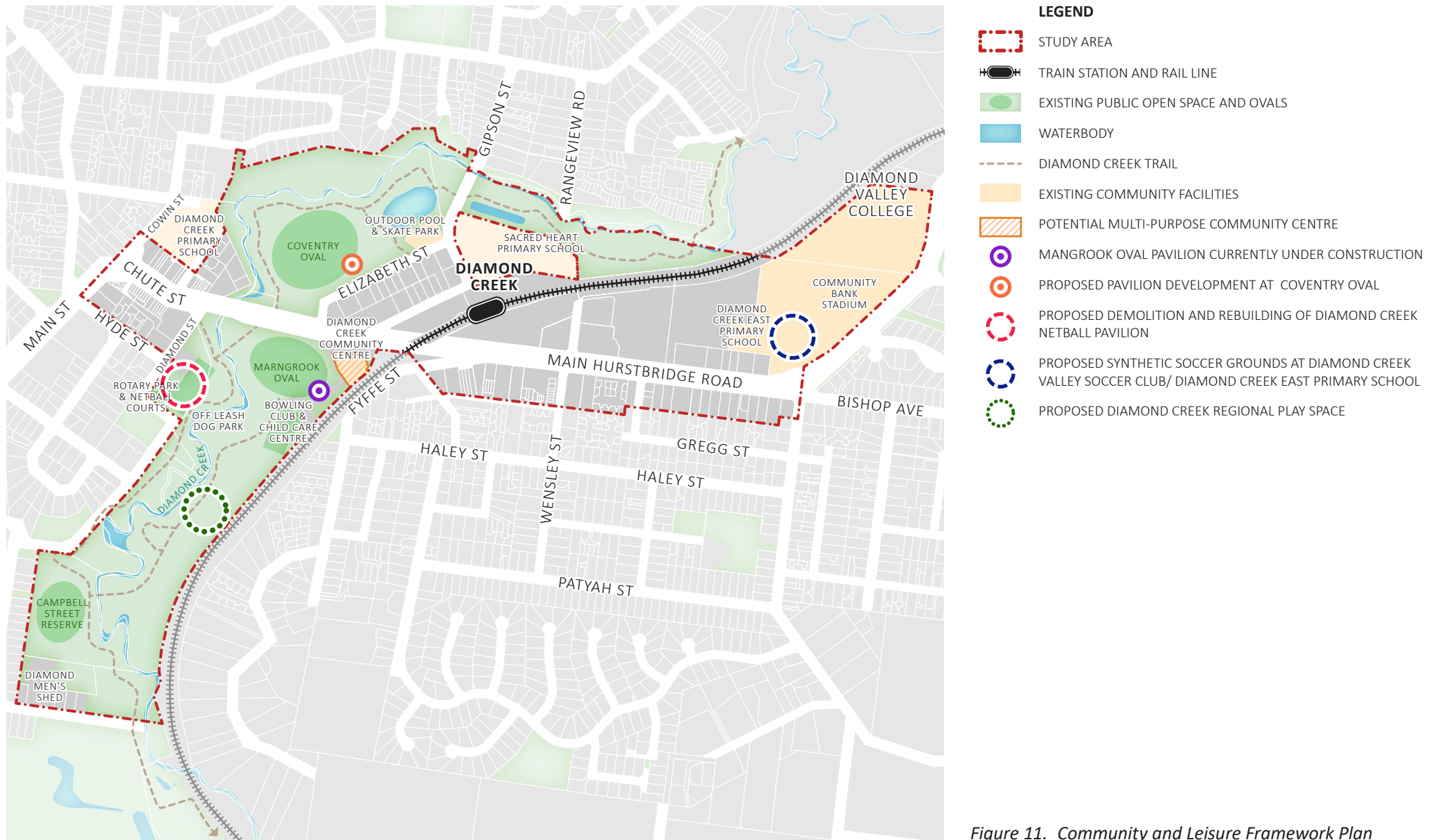


Figure 11. Community and Leisure Framework Plan

## 5.0 Precincts - Objectives, Strategies and Design Guidelines

The activity centre comprises seven precincts as shown at Figure 12. These precincts are each envisaged to have a unique future character that is distinctive yet complementary to one another. Articulated in the following sections is the preferred character, objectives and strategies as they relate to each precinct.

Each section is further supported by design guidelines that relate to specific design responses needed within each precinct.

For general design guidelines in relation to built form heights and setbacks, refer to Section 6.0.



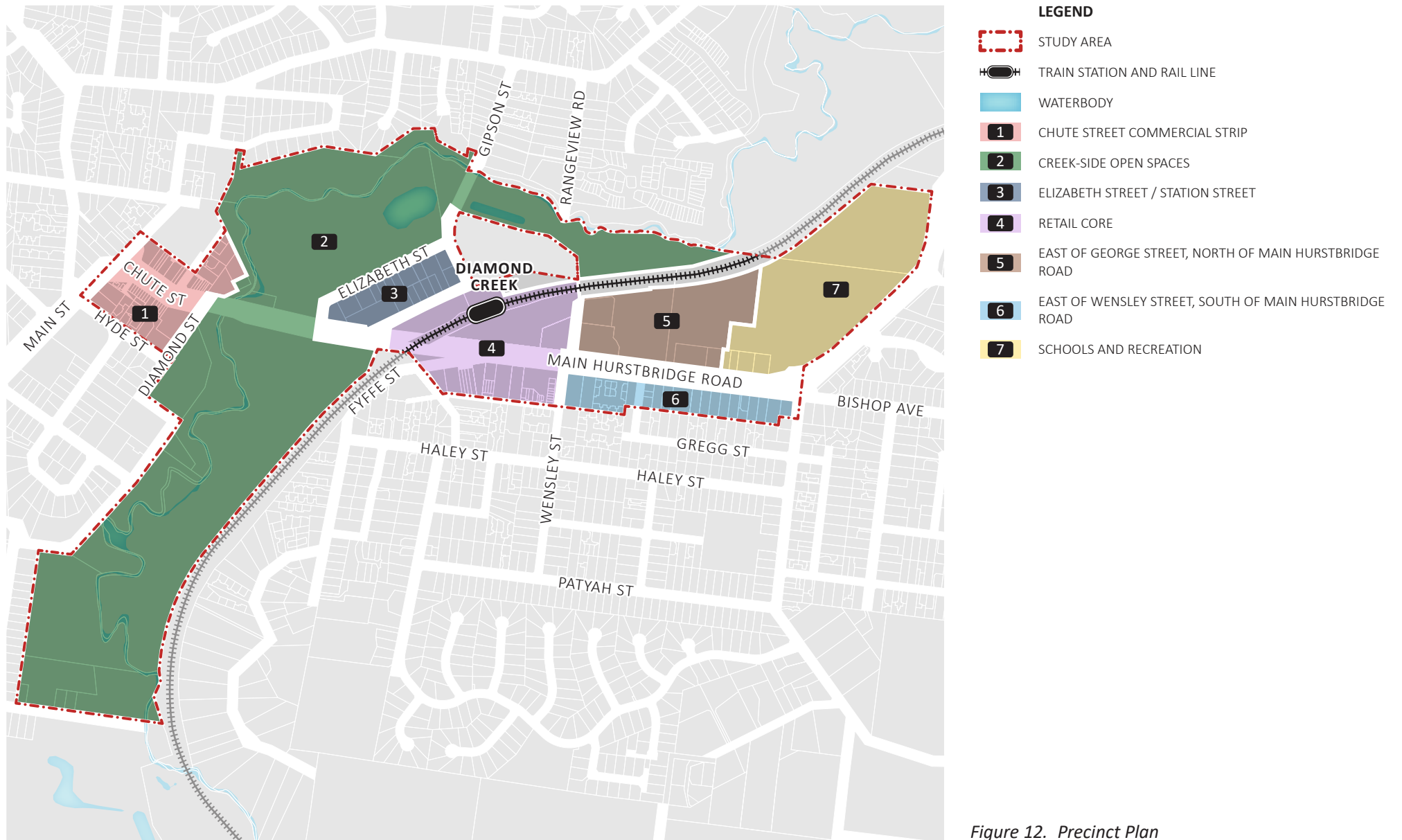


Figure 12. Precinct Plan

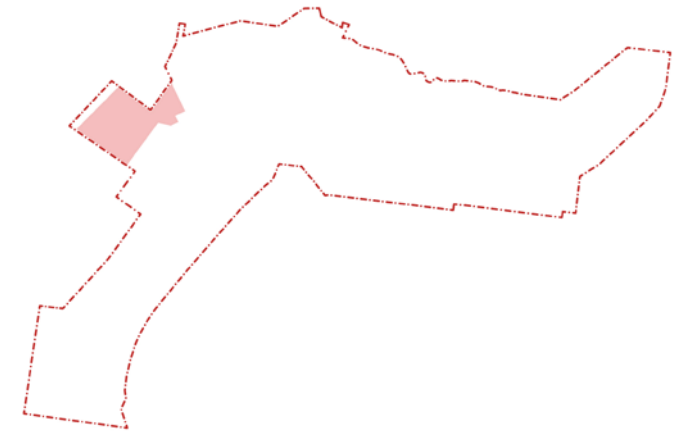
## 5.1 Precinct 1 – Chute Street Commercial Strip

### Preferred character statement

*Chute Street is the gateway to Diamond Creek from the west, where sweeping views of the valley are afforded from the hilltop location. Chute Street has a distinct and highly valued character within Diamond Creek, retaining its original small township atmosphere. New buildings will be moderate scale with a consistent street wall. The range of shop-fronts, cafes, places along the street to stop and sit and established trees will make an attractive pedestrian-oriented environment. Buildings will step down to the corridor of creek-side parkland and offer a pleasant aspect to enjoy this natural setting.*

### Objectives

- To respect the historic role and character of Chute Street as the earliest part of the town centre as a distinguishing feature and attraction for visitors.
- To encourage moderate density mixed use development that is responsive to the existing scale.
- To respond to the distinct fine grain character.
- To encourage buildings to step down the slope providing a link and orientation to the natural settings and leisure opportunities along the creek valley.
- To ensure retail, office and entertainment activities are provided as a secondary node to the retail core (Precinct 4), providing local convenience shopping.
- To protect the amenity of adjoining residential properties.
- To improve pedestrian and cyclist connectivity and amenity.
- To create a safe, accessible and attractive pedestrian environment activated through a range of shop-fronts, cafes, businesses, places to sit and stop with established trees and other attractions.



### Strategies

- Develop the retail, office and entertainment activities in and around Chute Street for local convenience shopping, a range of food and restaurant outlets and professional and other office based services but little if any retail floor-space expansion.
- Encourage higher density development in and around the Chute Street node.
- Investigate the implementation of on-road cycling lanes on Chute Street.
- Introduce flashing lights at the Cowin Street school crossing to increase visibility.

### Design Guidelines

- Buildings fronting Chute Street should:
  - Be designed with well-articulated facades, roof forms, fenestration, parapet treatments and other detail and materials to provide interest at the street level and reinforce the human scale of Chute Street.
  - Provide active frontages at ground level to provide a safe and lively environment.
  - Provide continuous weather protection (such as cantilevered canopies/verandahs) over footpaths, subject to protecting existing tree canopies.
  - Include a street wall that includes both a fine grain and strong vertical articulation to deliver visual interest.
- Buildings on corner allotments should address both street frontages with shop front windows at street level.
- Buildings should respond to the topography ensuring the front door to the ground floor use is at the same level as the footpath.

- Development on lots directly interfacing with the creek should be designed to address the creek with entries, windows and balconies.
- Development fronting Hyde Street and Inglis Street should adopt consistent setback distances to adjacent buildings and seek to retain any existing vegetation along property boundaries.
- Development on Inglis Street and Hyde Street should respond to the surrounding residential use, scale and intensity.
- Redevelopment adjacent to 69 Main Street should be designed to reflect and respond to its heritage features.

Refer to the General Guidelines at Section 6.0 for interface setback requirements.

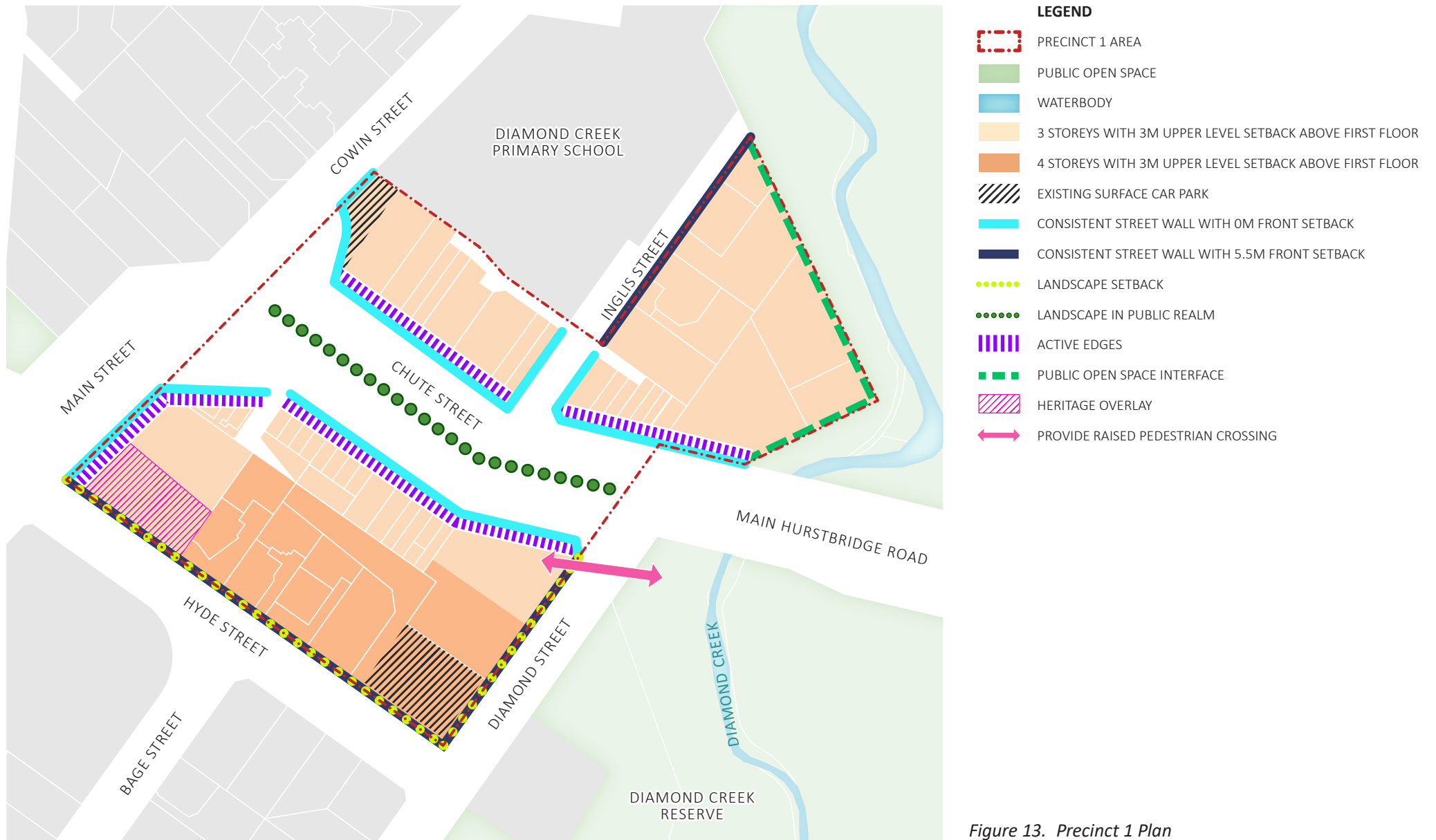


Figure 13. Precinct 1 Plan



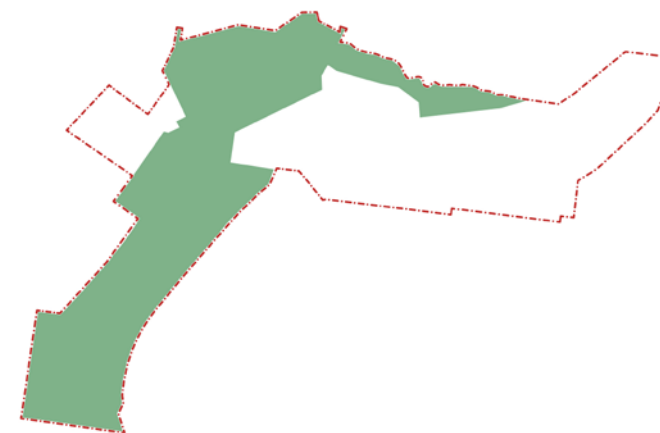
## 5.2 Precinct 2: Creek-side Open Spaces

### Preferred character statement

*The creek-side open space corridor is the most significant landscape and recreational feature in the township, and is central to the image and character of Diamond Creek. Located in the physical centre of the town, the corridor will be conveniently accessed from on-road cycle lanes connecting to the Diamond Creek Trail. The corridor will be known for catering for a diverse array of recreational activities including physical exercise, dog walking, picnics and play in spaces that cater for all ages and disabilities. The natural beauty of the creek corridor will extend into adjoining open spaces with native trees, shrubs and grasses dispersed amongst the recreational facilities, while the use of exotic species such as the avenue of elm trees at Marngrook Oval, will continue to provide a link to European settlement in the township.*

### Objectives

- To ensure the extent and nature of development should be limited and directly related to parkland based activities and promote use of the public parkland space.
- To provide a mix of active and passive recreation activities for everyone, regardless of age or disability.
- To provide safe crossing of Main Hurstbridge Road for pedestrians and cyclists.
- To enhance both the functional and recreational access for pedestrians, cyclists and park users, in particular through an improved path network.



## Strategies

- Advocate for reduced speed limits in Main Hurstbridge Road.
- Investigate the capacity of the existing Diamond Creek Community Centre to be refurbished to be a multi-purpose community facility that includes a variety of services and spaces for all age groups and potentially a library.
- Continue to maintain and improve existing recreational and community facilities within the Diamond Creek Valley.
- Review provision of sports ovals, pavilions, netball, tennis and bowls facilities to ensure they are compliant with current standards.
- Investigate the potential for an underpass connecting the Diamond Creek Trail under Main Hurstbridge Road.
- Implement proposed bike path connecting the Diamond Creek Trail to Hurstbridge to the north east.
- Develop a prioritised program for construction of functional and recreational paths as shown on the Figure 14.

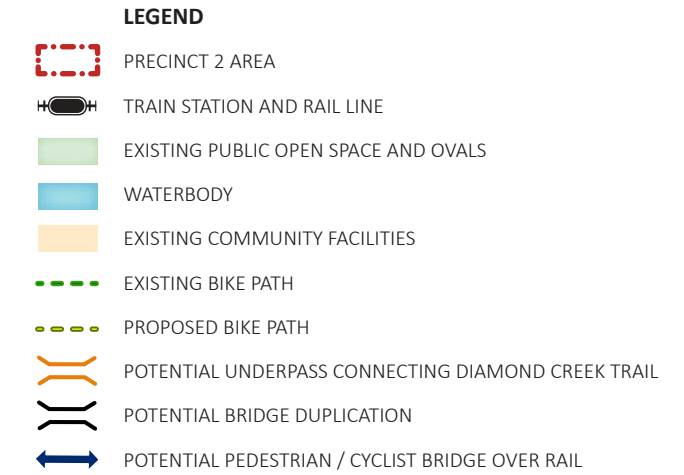


Figure 14. Precinct 2 Plan

## 5.3 Precinct 3: Elizabeth Street/ Station Street

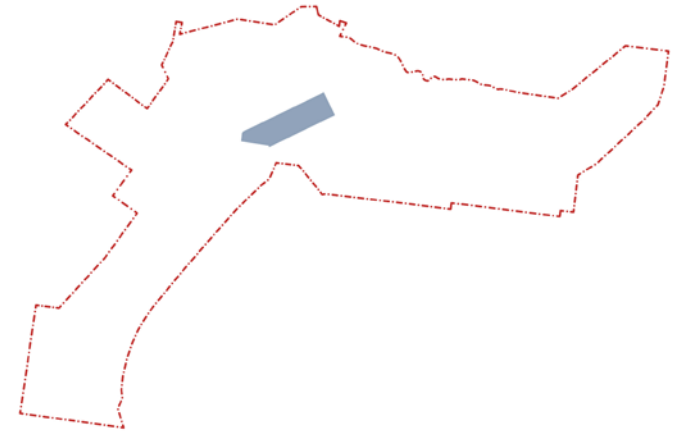
### Preferred character statement

*This area will become a truly mixed use precinct, where the existing local trades and services can continue alongside other emerging industries such as start up businesses, small-scale manufacturers and technology oriented start-up businesses, office and creative industries. The emerging economy will help to develop an innovation hub in Diamond Creek. At the same time, a limited amount of residential will be allowed to complement the desired economic outcome of the locality, respond to its location next to the train station, and provide activation, amenity and support new types of live-work development.*

*The buildings will present a cohesive image for the area with consistent setbacks and heights and will be articulated to break up the bulky forms usually seen in service industrial areas. A theme of native vegetation within front setbacks along Elizabeth Street will link this area to the adjoining parklands.*

### Objectives

- To support existing industrial uses whilst facilitating other compatible emergent industries to energise the centre.
- To minimise the impacts of development on adjoining sensitive land uses.
- To encourage high density mixed use development.
- To create a cohesive image for the area and encourage contemporary built form responses which provide interest to the streetscape and contribute to an attractive pedestrian environment.
- To emphasise the Main Hurstbridge Road frontage with buildings which exemplify the overall character and image of Diamond Creek.
- To create an native planting theme within front setbacks along Elizabeth Street which will link this area to the character of adjoining parklands.
- To improve the public realm and minimise conflicts between cars and pedestrians.



## Strategies

- Review zoning and consider application of a Commercial 3 Zone (C3Z) (or modified ACZ2) to facilitate creative industries, small manufacturers, start-up businesses and residential.
- Change the functions along the Main Hurstbridge Road frontage to intensify the land use and increase the amenity along this spine of the activity centre.
- Investigate the opportunity to make Station Street one way, to lessen conflicts between cars parking and pedestrians and reducing vehicular turning movements, minimising congestion.
- Investigate implementation of indented parking and a footpath on the western side of Elizabeth Street.
- Implement Gipson Street bridge duplication as specified in DCPO1, to improve traffic flow to the Activity Centre. As part of this, investigate complementary measures to:
  - Improve the amenity of the bridge as an entrance into the centre.
  - Enhance the connectivity the bridge provides for pedestrians/ cyclists in and out of the centre (e.g. clear provision for cyclists and pedestrians on the bridge).

- Provide a safe and non-congested environment in the Station Street precinct, particularly by considering traffic calming devices, pedestrian crossings, carefully-placed parking, etc.

## Design Guidelines

- Any future redevelopment of the Diamond Creek Tavern should be reflective of its heritage features and mark the entry to the Diamond Creek Major Activity Centre.
- Redevelopment adjacent to the Diamond Creek Tavern should ensure it does not dominate the heritage form.
- Built form should present an attractive and articulated façade to provide visual interest.
- Limit convenience parking areas to front setbacks landscaped to screen parking areas.
- Developments should not provide storage areas within the frontage.
- Front fencing along the street boundary is highly discouraged.

- Buildings should be designed with active frontages (greater than 60% non-reflective glazing) at ground level to provide a safe and lively environment.
- Buildings should present an attractive and articulated façade to provide visual interest to the overall development and offset bulky forms to the rear.

Refer to the General Guidelines at Section 6.0 for interface setback requirements.



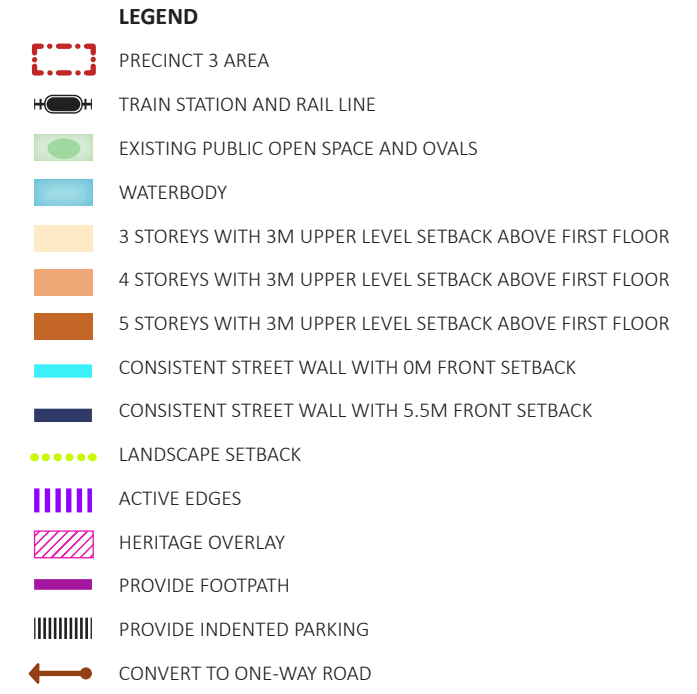
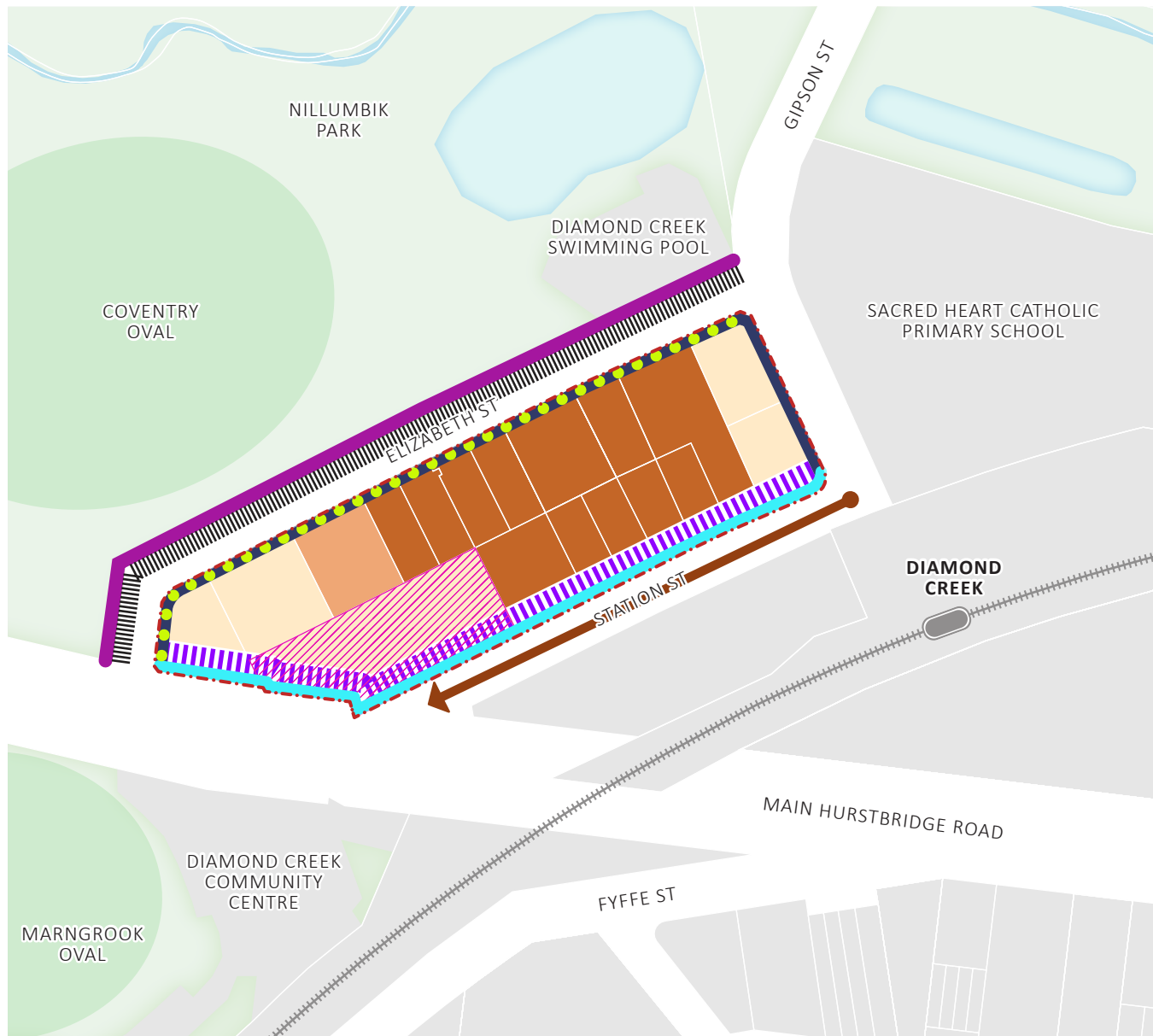


Figure 15. Precinct 3 Plan

## 5.4 Precinct 4: Retail Core

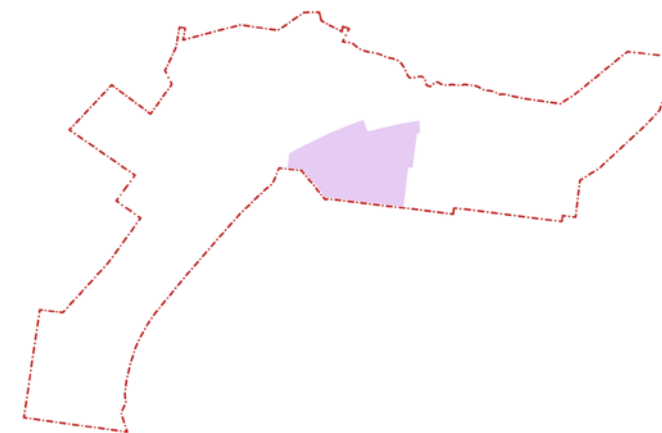
### Preferred character statement

*This area will consolidate as the major focus and ‘heart’ of Diamond Creek Activity Centre, comprising a range of uses including shops, offices and housing. The attractive shop fronts and outdoor cafes will create a buzz of activity at the Main Hurstbridge Road frontage. The land between the rail line, Main Hurstbridge Road and George Street should become the focus of activity, with a public space/plaza for people to stop, sit and meet friends. An internal public street with fine grain shops and residential development above will create energy and vitality.*

*The street spaces will be landscaped with native and exotic vegetation and provide attractive links for pedestrians moving between the shops and the station. Buildings will be designed to have active frontages with highly articulated facades. They will maintain the existing low scale of Diamond Creek and provide additional interest at the street level.*

### Objectives

- To support growth in retail floorspace.
- To enable mixed use, accessible development and housing including accessible housing options, shops, offices and housing.
- To promote the creation of a public space/plaza in the centre.
- To create a town centre ‘heart’ away from the traffic congestion on Main Hurstbridge Road.
- To retain the township atmosphere of this precinct while allowing opportunities for increased development (including transit oriented development) on large sites close to the train station.
- To link the various activities of the precinct visually and physically with contemporary building forms, public spaces and an attractive, continuously accessible pedestrian environment.
- To create a ‘landscaped civic spine’ along Main Hurstbridge Road as the connecting feature of Diamond Creek.
- To create a stronger presence of built form and activity along Main Hurstbridge Road.
- To create improved pedestrian connectivity between both sides of Main Hurstbridge Road and the rail line.
- To advocate for the removal of the level crossing and the implementation of a sky rail as per the general concept provided at Figure 8.
- To develop the role of the station as a sustainable transport hub ensuring it is clearly accessible from the retail core.
- To encourage traffic calming measures to Main Hurstbridge Road to improve conditions for pedestrians and cyclists.
- To develop a long-term plan for car parking including designated accessible parking bays to ensure an adequate and sustainable supply.



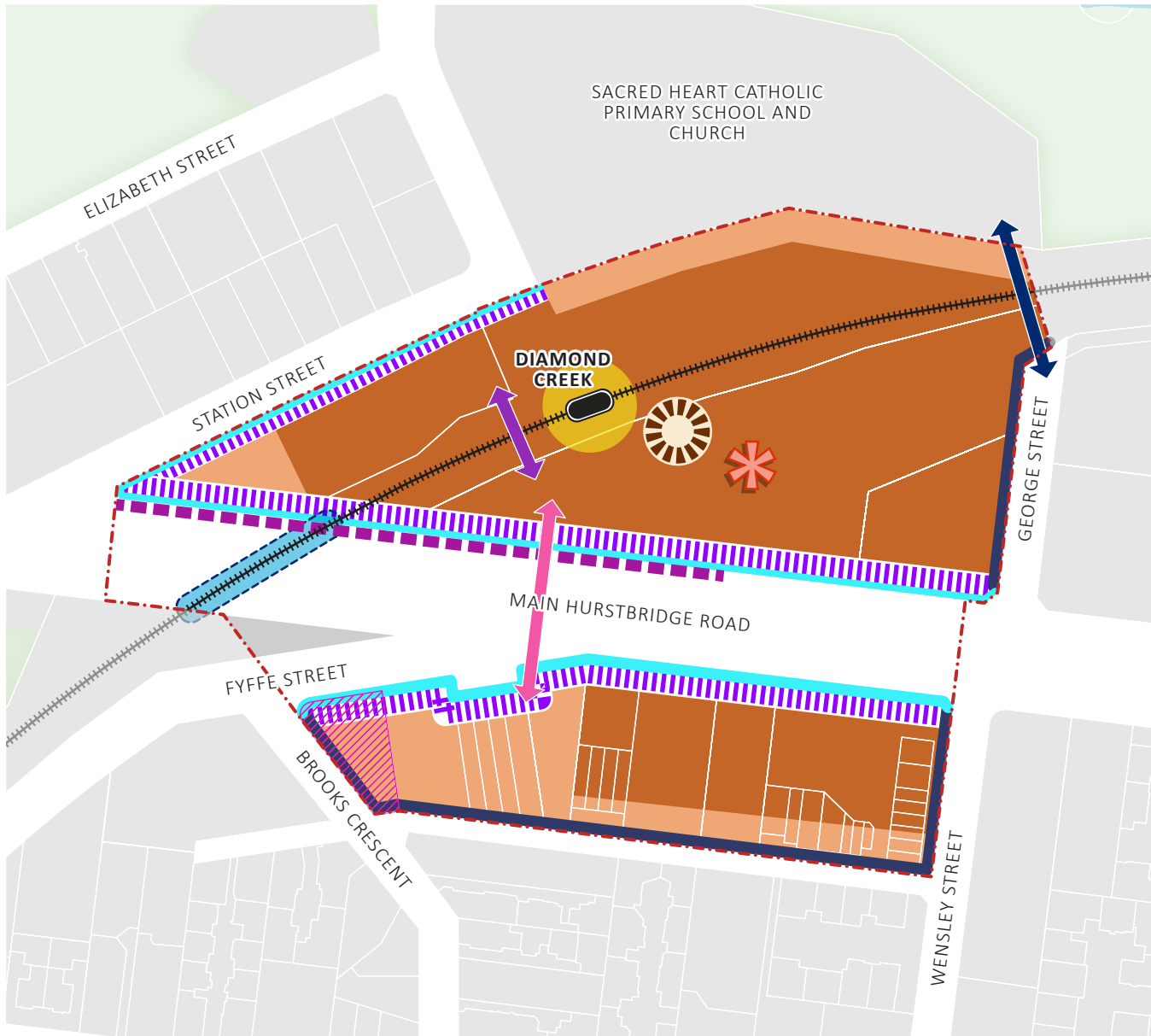
## Strategies

- Develop this precinct as the primary focus for retail activity in the centre.
- Generally contain retail activity within a compact pedestrian friendly core bound by the railway, Brooks Crescent, Waigo Way and Wensley Street/ George Street.
- Provide new retail and office space through more intensive, mixed use redevelopment.
- Deliver a vertical mix of land uses through facilitating the use of shop tops for office, residential and other commercial uses within the main streets of the activity centre.
- Encourage mixed use residential and office buildings north of the railway.
- Provide the opportunity for a transit-oriented development (TOD) on land to the west of the railway line (Vic Track land) including uses such as commercial and residential.
- Encourage progressive upgrades of railway infrastructure and the station to meet anticipated growth in demand for public transport services (including implementation of a potential sky rail).
- Liaise with the DoT to advocate for the removal of the level crossing including confirmation of the preferred outcome.
- Investigate redeveloping at-grade car parks for mixed use development, with replacement and additional car parking (if required) integrated in basement car parking.
- Investigate the opportunity for a raised pedestrian crossing directly east of Fyffe Street, connecting the north and south sides of Main Hurstbridge Road.
- Investigate improvements to station access and visibility, creating an inviting entrance.
- Investigate the provision of additional pedestrian railway crossings to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond.
- Investigate potential footpath upgrades and widening along Main Hurstbridge Road.
- Implement improvements to the existing railway pedestrian crossing.
- Reduce the speed limit along Main Hurstbridge Road to 40km/h.
- Provide bike parking at highly frequented facilities such as supermarkets and the train station.
- Liaise with the DoT to improve existing bus stops along Main Hurstbridge Road.
- Liaise with DoT to advocate for the implementation of on-road cycle lanes on Main Hurstbridge Road.
- Redevelopment should respond to the distinct fine grain character along the southern side of Main Hurstbridge Road.
- Prepare an Urban Design Framework (UDF) that incorporates a public space/ plaza and the 'heart' of the Activity Centre to the north of Main Hurstbridge Road away from traffic congestion.

### Design Guidelines

- Buildings fronting Main Hurstbridge Road should:
  - Be designed with well-articulated facades, roof forms, fenestration, parapet treatments and other detail and materials to provide interest at the street level.
  - Be built to the front boundary, with car parking (including designated accessible parking bays) behind.
  - Provide active frontages at ground level to provide a safe and lively environment.
  - Provide continuous weather protection (such as cantilevered canopies/verandahs) over footpaths.
  - Include a street wall that includes both a fine grain and strong vertical articulation to deliver visual interest.
- Buildings on corner allotments should address both street frontages with shop front windows at street level.
- Design new buildings with the ground floor located responding to the natural ground level (NGL) to promote a strong connection with the public street space and ensure access to all.
- Plant substantial native and exotic trees where appropriate to provide shade in car parks and screen car parking areas.
- Front and side fencing is strongly discouraged.
- The use of roller shutters on shopfront windows and doors at street level is strongly discouraged.
- If required, at-grade or deck car parking is encouraged behind developments with an emphasis on landscaping to minimise visual impact on adjoining residential areas.
- New development at 42 and 44 Main Hurstbridge Road should provide for the retention and improvement of the site as a local landmark building.

Refer to the General Guidelines at Section 6.0 for interface setback requirements.



- LEGEND**
- PRECINCT 4 AREA
  - TRAIN STATION AND RAIL LINE
  - EXISTING PUBLIC OPEN SPACE
  - WATERBODY
  - 4 STOREYS WITH 3M UPPER LEVEL SETBACK ABOVE FIRST FLOOR
  - 5 STOREYS WITH 3M UPPER LEVEL SETBACK ABOVE FIRST FLOOR
  - CONSISTENT STREET WALL WITH 0M FRONT SETBACK
  - CONSISTENT STREET WALL WITH 5.5M FRONT SETBACK
  - LANDSCAPE IN PUBLIC REALM
  - ACTIVE EDGES
  - HERITAGE OVERLAY
  - POTENTIAL LEVEL CROSSING REMOVAL
  - POTENTIAL TRAIN STATION FACILITIES IMPROVEMENT
  - EXISTING RETAIL ANCHOR
  - POTENTIAL PUBLIC PLAZA
  - IMPROVE EXISTING PEDESTRIAN CONNECTION
  - INTRODUCE PEDESTRIAN/ CYCLIST BRIDGE OVER RAIL
  - PROVIDE RAISED PEDESTRIAN CROSSING
  - PROVIDE FOOTPATH

Figure 16. Precinct 4 Plan



## 5.5 Precinct 5: East of George Street, North of Main Hurstbridge Road

### Preferred character statement

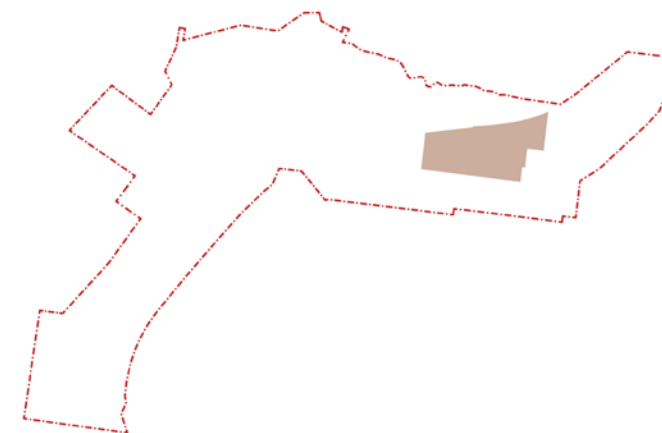
*As the approach to the Diamond Creek town centre from the east, the larger scale civic and commercial buildings, set in well-landscaped surrounds, will retain the rural township character.*

*This area will become a truly mixed use precinct, where the existing industrial uses and bulky goods can continue alongside other start up businesses, small-scale manufacturers and technology oriented start-up businesses, office and creative industries.*

*Re-established native vegetation along this stretch of Main Hurstbridge Road will create a distinct character for the area and make a welcoming statement at the town entrance. In addition, heavily landscaped property frontages will also provide visual continuity.*

### Objectives

- To encourage a range of industrial, commercial, office, civic and other employment generating uses complementing the use mix in Precinct 4.
- To minimise off site effects on adjoining sensitive land uses.
- To maintain the 'rural periphery' and landscape setting of this section of Main Hurstbridge Road.
- To enhance the strong native vegetation theme of this part of Main Hurstbridge Road, as the defining feature of the eastern entrance to the township.
- To encourage an attractive and safe environment for pedestrians and cyclists.



## Strategies

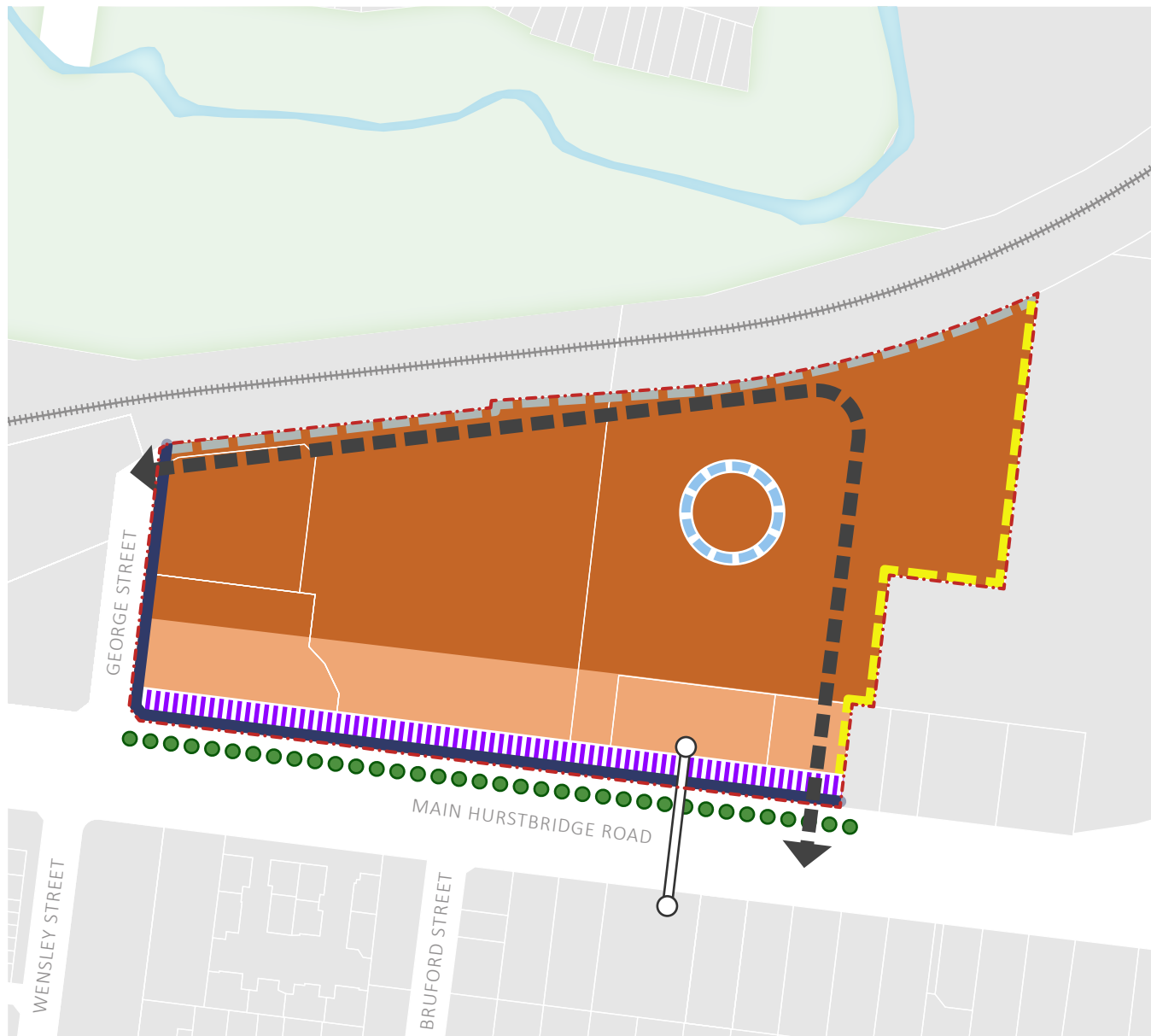
- Undertake community consultation to review the potential application of C3Z (or modified ACZ2) to part of Precinct 5 to allow for a diversification of uses and retention of the existing industrial uses.
- Investigate potential to establish an employment anchor, such as a civic or health anchor.
- Continue to support retail or semi-retail development that requires larger floor spaces, trade supplies or bulky goods.
- Maintain a suitable landscape buffer treatment along the eastern precinct boundary to act as a buffer to adjacent sensitive land uses.
- Retain landscaping along the frontage to Main Hurstbridge Road.
- Investigate potential footpath upgrades and widening along Main Hurstbridge Road, subject to protection of existing trees in the public realm.
- Reduce the speed limit along Main Hurstbridge Road to 40km/h.
- Liaise with DoT to advocate for the implementation of on-road cycle lanes on Main Hurstbridge Road.

## Design Guidelines

- Buildings fronting Main Hurstbridge Road should:
  - Be setback 5.5m from front boundary.
  - Provide active frontages at ground level to provide a safe and lively environment.
  - Be designed with ground floor street frontages with windows with displays and doors at the street level.
- Locate habitable spaces on upper levels along the street frontage, with windows overlooking the street or parklands/railway corridor to create a sense of surveillance.
- Locate storage yards, loading docks and large car-parking areas to the side or rear of the buildings to reduce their visual prominence.
- Locate main building entrance on street frontage so it is visible and easily recognised.
- Larger scale commercial and industrial buildings to be set in well landscaped surrounds to contribute to the rural township character of the centre.

- Provide space in front setbacks for adequate landscaping including indigenous grasses, shrubs and canopy trees and sufficient permeable surface to ensure plant growth.
- Minimise the number of driveway cross-overs to improve footpath safety and accessibility and opportunities for on-street parking.
- All industrial sheds should include a glazed office/display component to provide an interface with the street and to assist in concealing the bulky form behind.
- Front fencing along the street boundary is strongly discouraged.

Refer to the General Guidelines at Section 6.0 for interface setback requirements.



## LEGEND

- PRECINCT 5 AREA
- RAIL LINE
- EXISTING PUBLIC OPEN SPACE
- WATERBODY
- 4 STOREYS WITH 3M UPPER LEVEL SETBACK ABOVE FIRST FLOOR
- 5 STOREYS WITH 3M UPPER LEVEL SETBACK ABOVE FIRST FLOOR
- CONSISTENT STREET WALL WITH 5.5M FRONT SETBACK
- LANDSCAPE IN PUBLIC REALM
- ACTIVE EDGES
- RAIL LINE INTERFACE
- INTERFACE WITH PUBLIC USE ZONE EDUCATION (PUZ2)
- ← - - - → POTENTIAL ROAD CONNECTION
- — ○ INTRODUCE PEDESTRIAN CROSSING WITH FLASHING LIGHTS
- OPPORTUNITY FOR ANCHOR, E.G. HEALTH OR CIVIC

Figure 17. Precinct 5 Plan

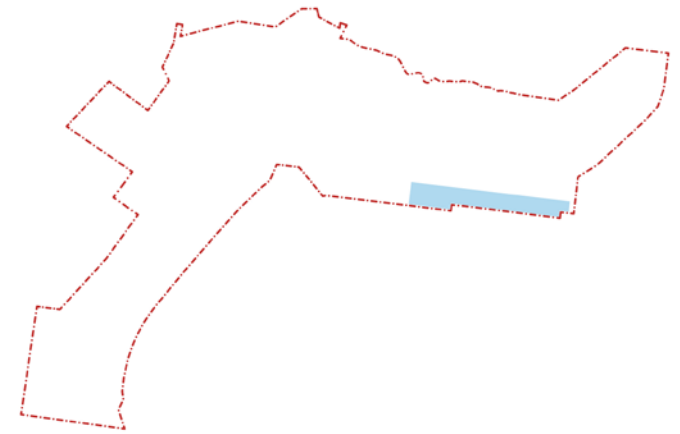
## 5.6 Precinct 6: East of Wensley Street, South of Main Hurstbridge Road

### Preferred character statement

*Although there will be mix of land uses in this area, including offices and professional/medical services, the buildings will remain residential in appearance because of their form, heights and setbacks. When viewed from Main Hurstbridge Road, there will be a strong presence of native vegetation including, trees, shrubs and grasses located in the front and rear gardens.*

### Objectives

- To encourage development that integrates with the existing residential character of the area.
- To encourage more intensive development to provide increased floor-space and efficient use of land.
- To encourage new development to present a predominantly residential appearance in respect to form, heights, set-backs streetscape and landscape.
- To create a strong presence of indigenous vegetation on the main road.
- To protect and enhance the amenity within the activity centre and surrounding established residential neighbourhoods.



**Strategies**

- Encourage a mix of activities including higher density housing, office development and medical services along the Main Road frontage.
- Ensure redevelopment responds to the existing low-scale residential character to the south.
- Upgrade streets to provide footpaths along the Main Hurstbridge Road edge and provide protection to pedestrians.

**Design Guidelines**

- Buildings fronting Main Hurstbridge Road should:
  - Provide a 5.5m landscaped front setback.
  - Encourage an active street frontage at ground level.
  - Be designed to provide natural surveillance by addressing the street and overlooking the public realm.
  - Include building articulation to create a visually interesting development.
  - Locate the main building entrance on the street frontage, so that it is visible and easily recognised and provide access paths to building entrances.
- Buildings should be designed with a predominantly residential appearance with front side and rear setbacks to allow for substantial landscaping.

- Buildings should transition in height and scale downwards to the residential hinterland to the south.
- Landscaping in the front, side and rear setbacks should include indigenous grasses, shrubs, substantial trees and sufficient permeable surface to ensure plant growth. Car-parking should be provided in a basement or to the rear of the buildings to reduce their visual prominence.
- The number of driveway cross-overs should be minimised to improve footpath safety and opportunities for on-street parking.

Refer to the General Guidelines at Section 6.0 for interface setback requirements.














- LEGEND**
-  PRECINCT 6 AREA
  -  3 STOREYS WITH 3M UPPER LEVEL SETBACK ABOVE FIRST FLOOR
  -  4 STOREYS WITH 3M UPPER LEVEL SETBACK ABOVE FIRST FLOOR
  -  CONSISTENT STREET WALL WITH 5.5M FRONT SETBACK
  -  LANDSCAPE SETBACK
  -  ACTIVE EDGES
  -  DIRECT RESIDENTIAL INTERFACE
  -  RESIDENTIAL STREET INTERFACE
  -  INTRODUCE FLASHING PEDESTRIAN LIGHTS

Figure 18. Precinct 6 Plan

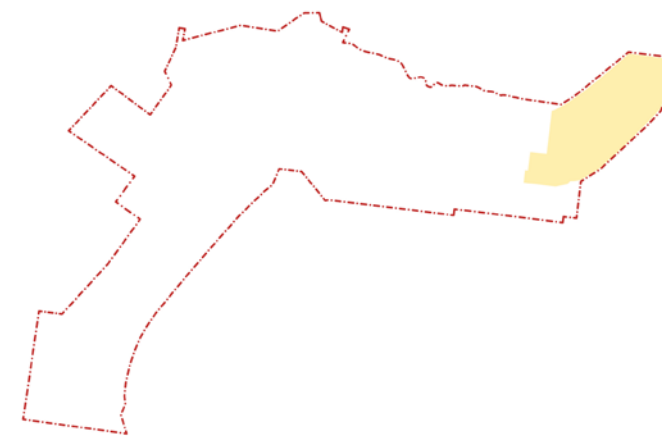
## 5.7 Precinct 7: Schools and Recreation

### Preferred character statement

*The recreational and school facilities in this area provide a strong focus for the local community. Buildings will be sited amongst the grounds of the school, in clusters, and designed with articulation on each visible façade. The landscaping on the edge of these sites will reflect and complement the strong native landscaping theme established along Main Hurstbridge Road. This will provide a highly distinct and attractive entrance to the Diamond Creek township from the east.*

### Objectives

- To consolidate and strengthen the range of educational, sporting and community activities.
- To encourage innovative and contemporary design forms that represent the local character of Diamond Creek and contribute to the sense of community in this area.
- To enhance the strong native vegetation theme of this part of Main Hurstbridge Road, as the defining feature of the eastern entrance to the township.
- To improve the access for pedestrians and cyclists in the precinct linking the recreational and school facilities to the north and the town centre in particular.



## Strategies

- Develop access to the precinct to link it with residential area to the north and east and to the town centre, in particular through shared pedestrian/cycle paths over the rail line.
- Plan for additional leisure, performing art and other complementary educational and community activities.
- Strengthen the landscape character along Main Hurstbridge Road through the provision of mature trees and planting within the streetscape and public realm.

## Design Guidelines

- Buildings should present articulated façades to the public realm.
- Buildings should be designed in contemporary architectural styles and include innovative responses that build on the local character of the area and complement the existing buildings.

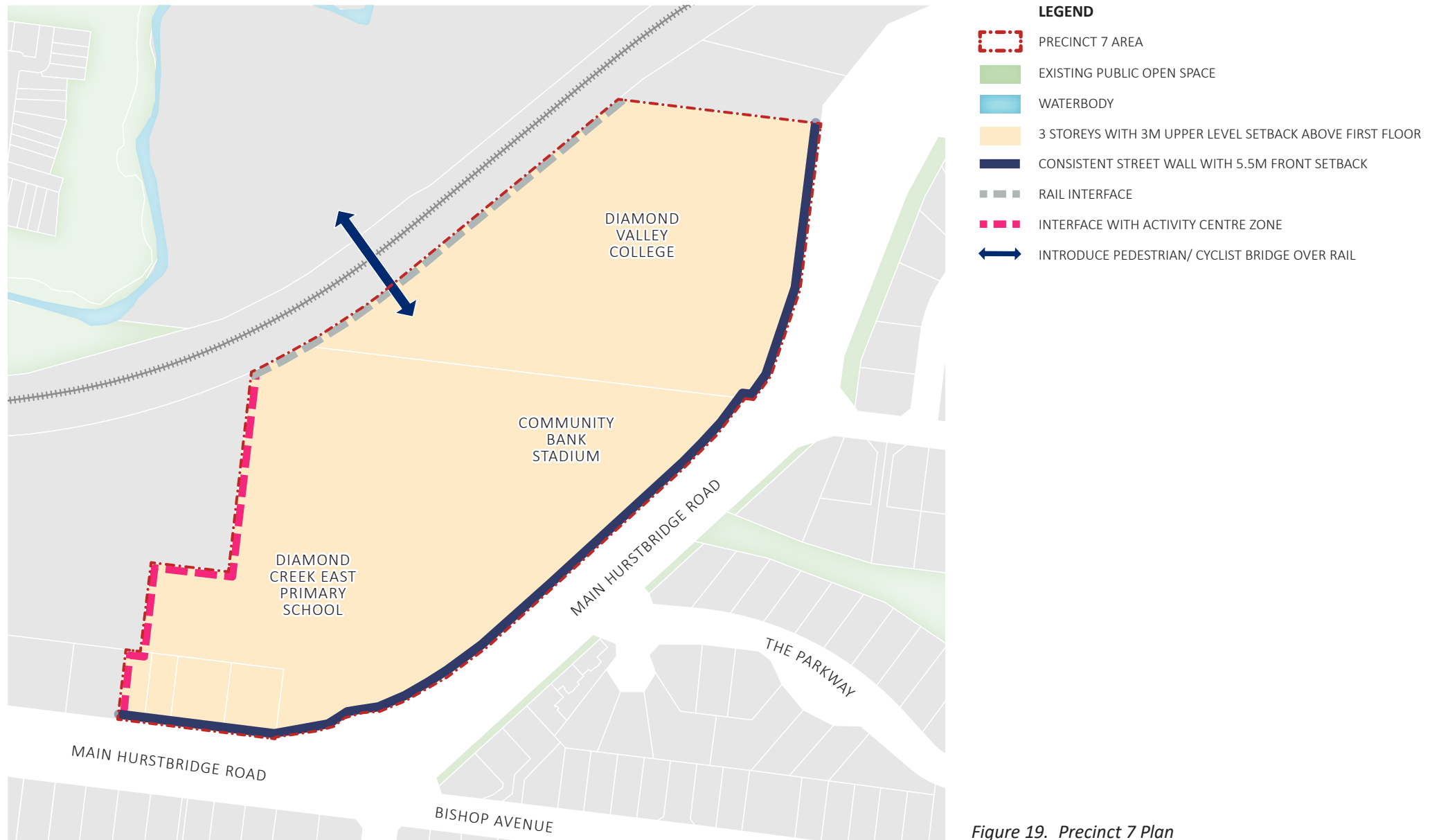


Figure 19. Precinct 7 Plan

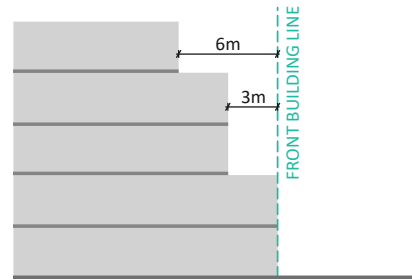
## 6.0 General Design Guidelines

The following built form guidelines provide the design parameters to achieve the Vision for the Draft Structure Plan, whilst also responding to design constraints associated with the public realm, residential interfaces, overshadowing and amenity.

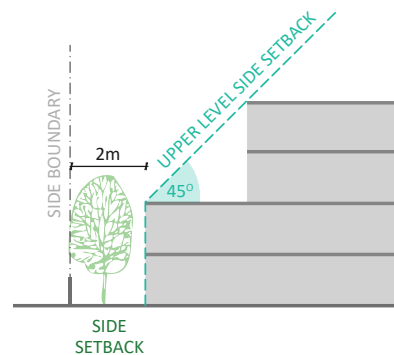
Apply the following design guidelines to development within the activity centre:

1. Building heights should not exceed the height specified in the precinct plans.
2. Development should include a front setback as specified in the precinct plans.

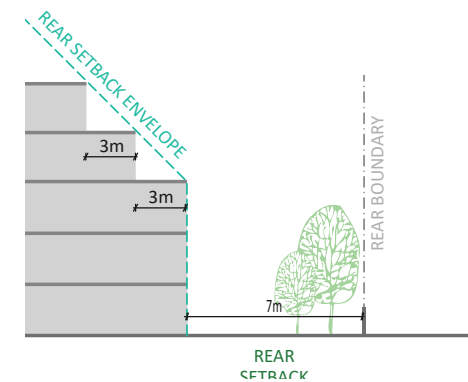
3. Any part of the building above 2 storeys should be:
  - Setback at least 3 metres from the front building line. Above 4 storeys the building should be setback a further 3 metres.



- Setback beyond a 45 degree plane from the sides of the level below (Precinct 6 and properties fronting Inglis Street only).



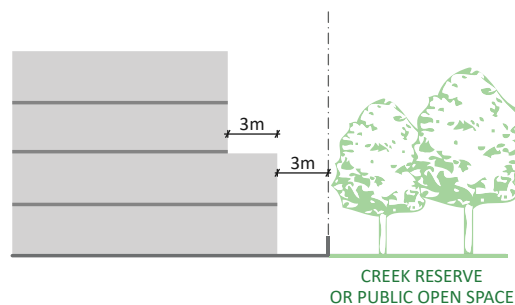
4. Rear setbacks to direct residential zones should be designed to manage visual bulk and overshadowing impacts to adjacent residential properties by:
  - Applying a 7m rear landscape setback; and
  - Apply a 1m setback for every metre of height over 10.9m.





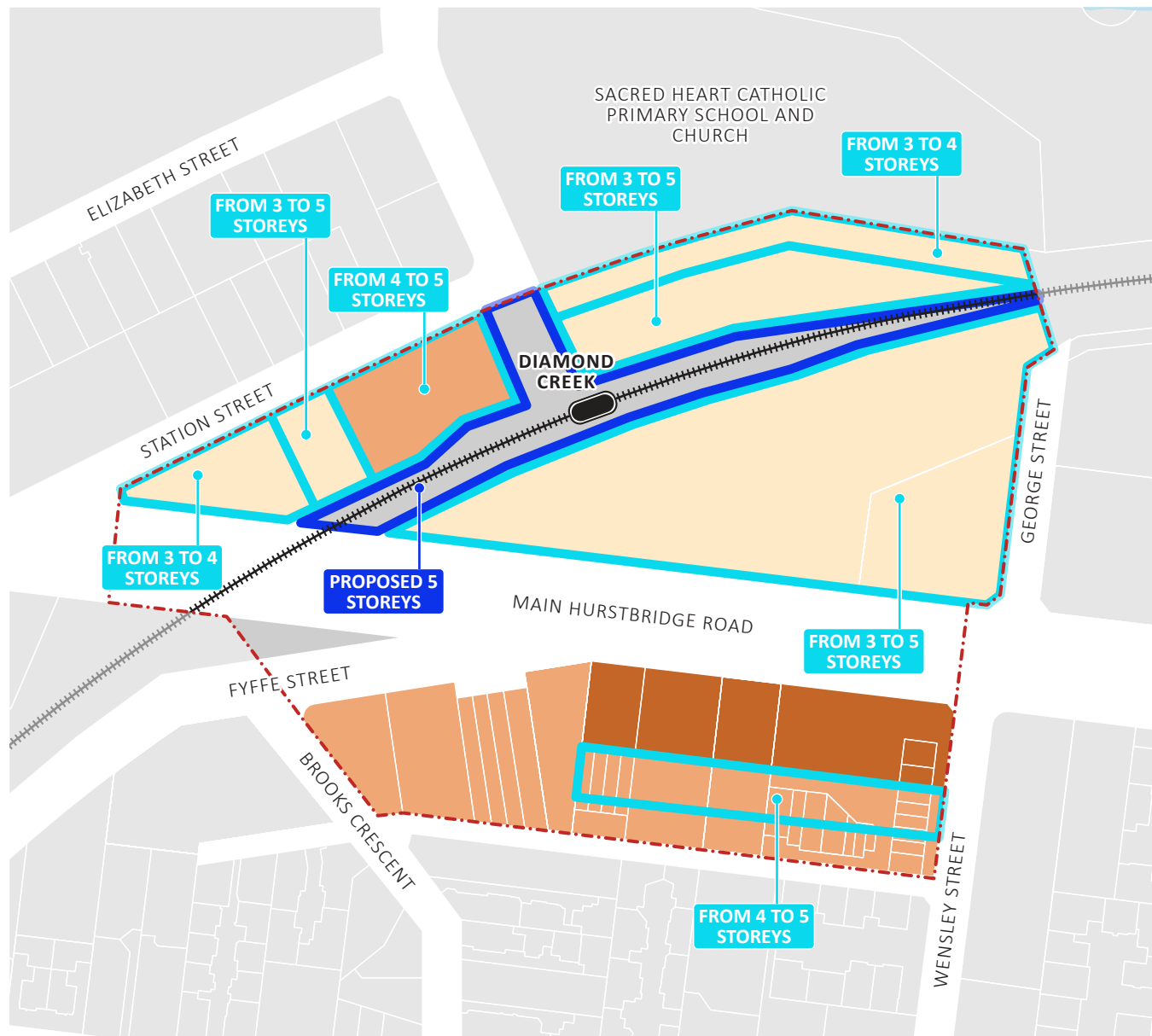
5. Rear setbacks to other zones above the street wall should be:
- 4.5m from the common boundary, where a habitable room window or balcony is proposed;
  - or
  - 3.0m from the common boundary where a commercial or non-habitable window is proposed.

6. Development on lots directly interfacing with Diamond Creek Reserve and open space should:
- Be designed to address the creek reserve with entries, windows and balconies.
  - Provide a 4 metre upper level setback above the street wall to the creek/open space.
  - Include a 3 metre landscaped setback from the property boundary facing the creek.
  - Incorporate a low, visually permeable fence to clearly define the public realm while retaining the potential for visual interaction.



**DRAFT**

**Appendix A:  
Existing and Proposed  
Height Differences**



## Precinct 4: Retail Core

### LEGEND







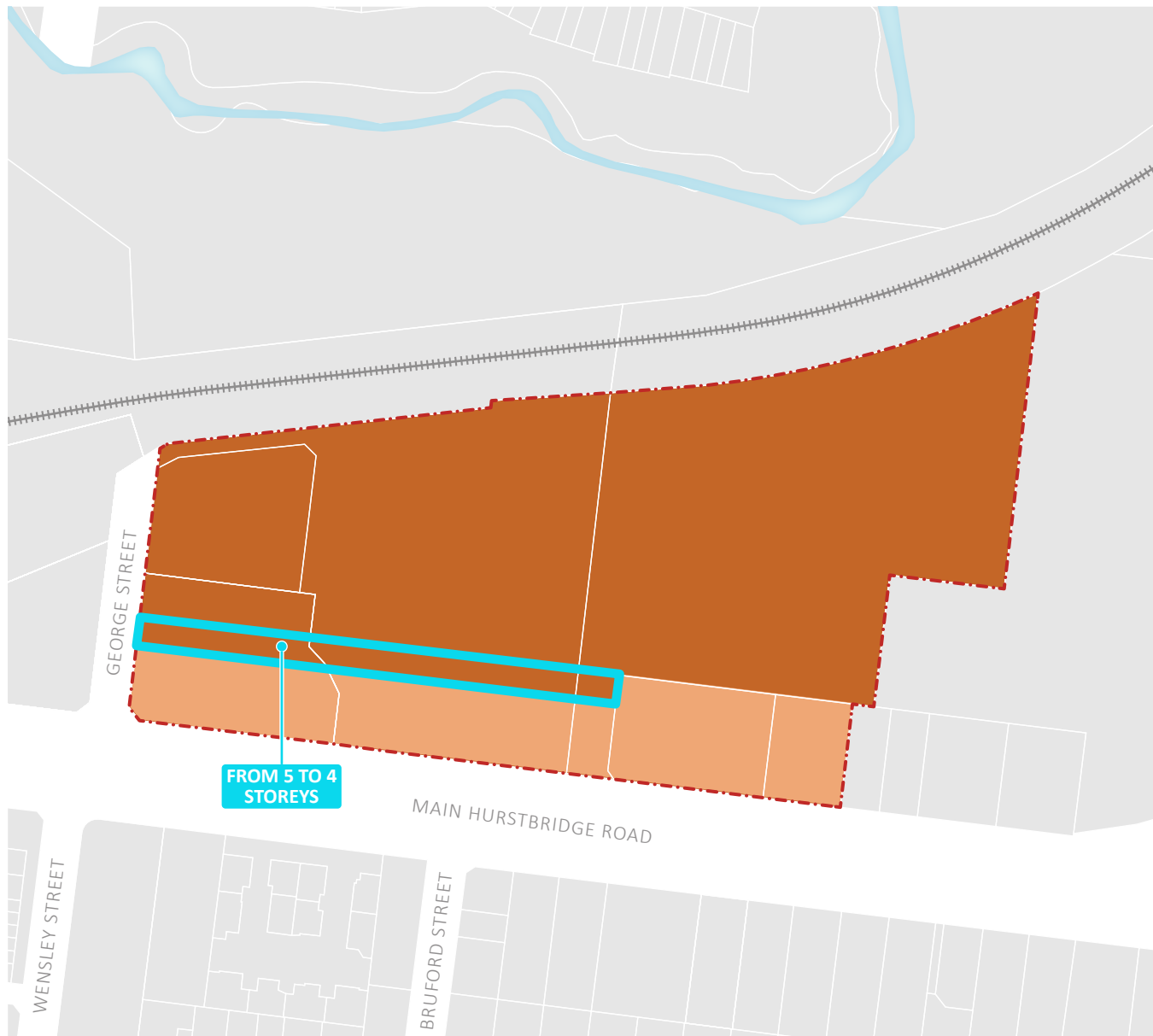
-  PRECINCT 4 AREA
-  TRAIN STATION AND RAIL LINE
-  EXISTING MAXIMUM HEIGHT OF 4 STOREYS
-  EXISTING MAXIMUM HEIGHT OF 5 STOREYS
-  PROPOSED CHANGE TO EXISTING MAXIMUM HEIGHT LIMIT
-  PROPOSED CHANGE TO NO EXISTING BUILDING HEIGHT RESTRICTIONS

Figure 20. Existing and Proposed Height Differences: Precinct 4



## Precinct 5: East of George Street, North of Main Hurstbridge Road

### LEGEND






-  PRECINCT 5 AREA
-  RAIL LINE
-  EXISTING MAXIMUM HEIGHT OF 4 STOREYS
-  EXISTING MAXIMUM HEIGHT OF 5 STOREYS
-  PROPOSED CHANGE TO EXISTING MAXIMUM HEIGHT LIMIT

Figure 21. Existing and Proposed Height Differences: Precinct 5

## Precinct 6: East of George Street, South of Main Hurstbridge Road



Figure 22. Existing and Proposed Height Differences:  
Precinct 6



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**Summary of REDACTED Submissions to the Diamond Creek Activity Centre Structure plan**

No.	Date received in SP inbox	Key Points
1	20 March 2020	<ul style="list-style-type: none"> <li>• Consider moving the pedestrian crossing in Chute Street to the end of Diamond Street</li> <li>• Re-open Hyde Street to allow another way for cars to flow</li> <li>• The path across the train line there needs to be much wider and maybe a zebra crossing over the side streets could help.</li> <li>• Precinct 1 - Good idea to increase the heights and encourage mixed use buildings taking into account Diamond Creek character</li> <li>• Precinct 2 – idea of old dam being used as tourist option</li> <li>• Supports strengthening landscape character – in particular the health of the River Red Gum in the carpark in Diamond Street as part of any development</li> </ul>
2	20 March 2020	<ul style="list-style-type: none"> <li>• Overall supportive of changes that enhances the semi-rural/heritage feel of the town centre.</li> <li>• Concerns over scale and heights of proposed development.</li> <li>• Concerns over proposed heights surrounding Diamond Creek Tavern.</li> <li>• Duplication of Main Road suggested.</li> <li>• Does not support Sky Rail.</li> <li>• Suggests the Senior Citizens building as a community hub location.</li> </ul>
3	22 March 2020	<ul style="list-style-type: none"> <li>• Wants to retain semi-rural character</li> <li>• Concerned about further degradation of environment and personal safety inherent in higher density residential development or any further mass housing estate development</li> <li>• Does not see the need for existing height levels and does not support any increases</li> <li>• Agree the development of a central plaza may be beneficial, but see no real need to increase the evening use of the area.</li> <li>• Agreement that level crossing removal required</li> </ul>
4	22 April 2020	<ul style="list-style-type: none"> <li>• Building any residential apartments here will be detrimental to the current residents and will impact semi-rural character</li> </ul>

		<ul style="list-style-type: none"> <li>• A revamp with more commercial spaces and a more modern feel, as well as removing the level crossing will be great</li> <li>• Concerns over road congestion</li> </ul>
5	23 April 2020	<ul style="list-style-type: none"> <li>• Traffic already congested</li> <li>• If trains increase frequency level crossing removal will be needed and this has to be done before further development</li> <li>• Increased car parking pressures on MHR and surrounding streets from further development</li> <li>• Loss of privacy on residential interfaces</li> </ul>
6	25 April 2020	<ul style="list-style-type: none"> <li>• Five level development would be an absolute eye-sore and create an over-crowded feeling</li> <li>• States the only businesses that seem to survive in Diamond Creek are predominantly food outlets and beauty services</li> <li>• Does not want Sky Rail – an underpass is better option</li> <li>• Does not agree with 40 kmh on MHR</li> <li>• Very positive regarding idea of library in Diamond Creek</li> <li>• If Station Street made one way then Elizabeth Street also be made one way</li> </ul>
7	26 April 2020	<ul style="list-style-type: none"> <li>• Opposed to all areas where multi-level development is proposed</li> <li>• Opposed to whole plan</li> <li>• Existing road congestion (and parking) will be made worse with development</li> <li>• Specifically mentions impact on amenity for Precinct 6</li> </ul>
8	27 April 2020	<ul style="list-style-type: none"> <li>• No five storey buildings, no sky rail</li> <li>• Doesn't believe level crossing removal required – widen MHR</li> <li>• Wants Diamond Creek to remain semi-rural town with green space and a homely feel</li> </ul>
9	27 April 2020	<ul style="list-style-type: none"> <li>• Opposes proposed potential development in precinct 1 states this would will ruin area and impact amenity for the residential</li> <li>• Supports local business development but at appropriate scale</li> <li>• Does not support raised walking track over Diamond Street</li> <li>• Supports the emphasis on improved lighting, footpaths, play and recreational activities</li> <li>• Does not support existing development heights in Precinct 3 – against additional residences in this area and stating road upgrades not identified</li> <li>• Extra parking on Elizabeth Street not considered adequate</li> </ul>

		<ul style="list-style-type: none"> <li>• A greater emphasis on more local businesses and variety is great - but not a scale this large.</li> <li>• MHR needs to have additional lanes especially due to bushfire risk</li> <li>• Questions what benefit residents would gain having a new Civic Building in Precinct 5 and how that would interact with the existing emergency services complex</li> <li>• Opposes the existing and proposed heights in Precinct 6 due to amenity issues and loss of privacy</li> <li>• Upgrade the roads and infrastructure before development occurs</li> </ul>
10	27 April 2020	<ul style="list-style-type: none"> <li>• Concerned about potential for 5 levels in Diamond Creek – in MHR it would completely destroy its of sense rural and bushland character</li> <li>• Community Hub needs to be a place that invites the public to enter</li> <li>• More spaces open for the public to play sport</li> <li>• Images in plan don't seem to show much vegetation – plant as much as possible and provide shade areas</li> </ul>
11	27 April 2020	<ul style="list-style-type: none"> <li>• Such dense construction will completely ruin the rural ambience of Diamond Creek</li> <li>• So much extra construction and no increase in road capacity would seem to be lunacy</li> </ul>
12	27 April 2020	<ul style="list-style-type: none"> <li>• Understands need for growth but concerned over scale - no 5 levels- small country town feel is what made him move to Diamond Creek</li> <li>• Area does need a plan and the road congestion needs addressed but it will be for nothing if Diamond Creek is over-developed</li> <li>• Wants railway crossing to be fixed</li> <li>• Doesn't agree with development in Precinct 6 - amenity impacts, increased parking pressures, increased congestion both on MHR and Gregg Street and believes this will kill atmosphere of Diamond Creek</li> </ul>
13	27 April 2020	<ul style="list-style-type: none"> <li>• People who live in Diamond Creek want the community feel and having space</li> <li>• Concerns over development without widening MHR – increase car parking pressures especially in Precinct 3 and 4</li> <li>• Does not agree with proposed potential development - shops should just be refurbished and remain one level to main the community feel.</li> <li>• Cannot believe that “the green wedge council” is planning on building 5 stories throughout the main areas of Diamond Creek!</li> </ul>
14	27 April 2020	<ul style="list-style-type: none"> <li>• Please do not ruin Diamond Creek with 5 story buildings and a sky rail - keep it low level, not intrusive</li> </ul>

		<ul style="list-style-type: none"> <li>• No to 5 story buildings. No to apartments and multistory dwellings and retail. Sounds very congested and claustrophobic</li> <li>• Concerned about loss of semi-rural feel – does not want development to the front boundary</li> <li>• Town square supported</li> <li>• Improved pedestrian/bicycle access generally supported especially at school area but reservations about pedestrian overpass in main town area</li> <li>• Supports creating open spaces for recreation and bike path and redevelopment of sporting facilities</li> </ul>
15	28 April 2020	<ul style="list-style-type: none"> <li>• Inappropriate for 4 &amp; 5 storey development in Diamond Creek</li> <li>• Not in favour of a transit oriented development that has residential above</li> <li>• Possibility of a sky rail makes sense</li> <li>• Design images in the plan need much more landscaping &amp; greening would be needed to make this a pleasant place to be.</li> <li>• Diamond Creek underpass supported</li> <li>• Supports the community themes in Theme 5</li> </ul>
16	29 April 2020	<ul style="list-style-type: none"> <li>• Revitalising the retail area, making Station Street one-way, increasing pedestrian and bicycle accessibility and retaining the natural feel of the area are all excellent ideas.</li> <li>• Visual pollution caused by 4 and 5 story buildings</li> <li>• Potential parking issues caused by increased residents</li> <li>• Vision of employment options in the area needs to be fleshed out</li> <li>• Higher density housing will also alter the feel of the village</li> <li>• Good thing to make car parks less obvious, while not losing capacity around Precinct 3 and 4</li> <li>• Supports 40 kmh on MHR</li> <li>• Supports level crossing but not sky rail</li> <li>• Supports one way system in Station Street</li> <li>• Gipson Street bridge should be kept as is</li> <li>• Supports the idea of more retail in Precinct 4 providing there is a plan in place to ensure development will be used</li> <li>• Retaining or increasing vegetation is a great idea, with the more natives the better</li> <li>• A public plaza is a good idea, as are better links to parkland and additional railway crossings for pedestrians and cyclists.</li> </ul>

17	29 April 2020	<ul style="list-style-type: none"> <li>• Plan aims to increase the retail landscape at the expense of what the area offers</li> <li>• Diamond Creek should look to capitalise on the independent traders</li> <li>• The employment anchor is a great idea</li> <li>• 5 storey developments on the main street will not encourage rural or bushland feel</li> <li>• Increased density is already happening in the area surrounding the shops (Haley street and surrounds) with existing blocks being subdivided into smaller block sizes - 250-350m - which is similar to inner city areas</li> <li>• Increased densities with limited road access increase difficulties in evacuation re bushfires</li> <li>• Supports level crossing removal but not sky rail</li> <li>• Supports more public spaces in the central area also better connections to other green areas</li> <li>• Supports increasing community spaces, upgrade of the Community centre and the idea of a library</li> <li>• Pedestrian crossing in Precinct 7 supported but questions student safety</li> </ul>
18	29 April 2020	<ul style="list-style-type: none"> <li>• Remove the level crossing (which we need) without giving yourself the authority to erect 5 story buildings</li> <li>• Diamond Creek provides adequate facilities and service for the size of the community</li> </ul>
19	29 April 2020	<ul style="list-style-type: none"> <li>• Plan focuses way too much on increased new development over simple and practical measures for improving and enhancing existing amenities</li> <li>• Buildings over 2-3 stories are incompatible with the existing rural village character of Diamond Creek</li> <li>• Does not support sky rail or duplication of Gipson Street bridge</li> <li>• Plan does not clearly explain solutions for improving traffic flow and car park layouts, along MHR and in Precinct 4</li> <li>• Supportive of ideas to improve bike paths, pedestrian paths, introduce cycle lanes and making Station Street one way</li> </ul>
20	29 April 2020	<ul style="list-style-type: none"> <li>• Some development would be great however it needs to be in step with the 'rural village charm'</li> <li>• The general employment objectives seem reasonable</li> <li>• Higher density living could destroy rural feel</li> <li>• Concerned about proposed potential development – no 5 levels with 3 at most</li> <li>• MHR should be duplicated before level crossing removal</li> <li>• Skyrail would be an eye sore and would totally erode why people live in diamond creek</li> <li>• Concerns over lack of car parking in relation to increased development</li> <li>• Supportive of the objectives and strategies relating to community and leisure including possible library</li> </ul>

		<ul style="list-style-type: none"> <li>• Gipson Street bridge must be duplicated to cope with traffic from existing new developments</li> <li>• Sky rails, 5 storey buildings and keeping the road 1 lane each way along the shopping precinct would achieve an undesirable outcome.</li> </ul>
21	24 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Concerned over height limits</li> <li>• Concerned over increased congestion</li> </ul>
22	26 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• MHR already heavily congested</li> <li>• Concerned about prospect of development to 4 levels on that stretch of road</li> </ul>
23	26 April 2020	<ul style="list-style-type: none"> <li>• Works in Elizabeth Street</li> <li>• Proposed development in the industrial and retail areas of the plan would take away semi-rural feel of Diamond Creek and block out light</li> <li>• Keen for level crossing removal</li> </ul>
24	27 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Lack of privacy for existing residential properties backing onto Precinct 6</li> <li>• Increased traffic , in general and in Gregg Street</li> <li>• Increased pressure on parking</li> </ul>
25	27 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Development on MHR will lead to loss of privacy</li> <li>• Increased noise</li> <li>• Potential overshadowing and loss of sunlight</li> <li>• Pressure on car parking</li> <li>• Specific concerns about increase traffic on Gregg Street</li> </ul>
26	27 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Existing car parking under pressure</li> <li>• Concerned about the scale and scope of potential development on MHR</li> </ul>
27	28 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Undertook letterbox drop of Gregg Street regarding the existing and potential development heights on MHR</li> <li>• 3-4 levels will not integrate into the existing character of Diamond Creek</li> </ul>



		<ul style="list-style-type: none"> <li>• MHR already too narrow and congested</li> <li>• Development in Precinct 6 will cause additional traffic particularly in Gregg Street</li> <li>• Existing issues at the Bruford and Gregg Street intersection</li> <li>• Believes appearance of apartments would be unsuitable for the area</li> <li>• Loss of privacy is a major issue</li> <li>• Suggests alternative locations better suited to multi-level development</li> </ul>
28	28 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Submission signed as Mitchell but in system as Dusing</li> <li>• Higher density both residential and commercial will remove the rural feeling</li> <li>• Roads are already congested – would require extra lanes – not having extra lanes would cause increased bushfire risk</li> <li>• Adding plaza and additional residential dwelling would bring Diamond Creek to a halt</li> <li>• Does agree with bicycle lane on MHR</li> <li>• Does agree with level crossing removal but needs to be in character with Diamond Creek</li> </ul>
29	28 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Higher density both residential and commercial will remove the rural feeling</li> <li>• Roads are already congested – would require extra lanes – not having extra lanes would cause increased bushfire risk</li> <li>• Adding plaza and additional residential dwelling would bring Diamond Creek to a halt</li> <li>• Does agree with bicycle land on MHR</li> <li>• Does agree with level crossing removal but needs to be in character with Diamond Creek</li> </ul>
30	28 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Development in MHR will undermine amenity that currently exists</li> <li>• Overshadowing and loss of privacy issues</li> <li>• Increased parking and traffic pressures</li> </ul>
31	28 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Area already crowded and busy</li> <li>• Overflow traffic coming into Gregg Street</li> <li>• Unique feeling of Diamond Creek will be lost</li> </ul>

32	29 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Changes would ruin feeling of small country town charm</li> <li>• MHR is only single lane and already has congestion issues</li> </ul>
33	29 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Already been an increase in traffic in Gregg Street due to dual occupancy sub-divisions in area</li> <li>• Development on MHR would increase congestion further</li> <li>• Existing issues at the Bruford and Gregg Street intersection</li> <li>• Development on MHR will cause loss of privacy</li> <li>• Anticipates increase in rubbish/litter</li> <li>• Multi-level housing should be put into newer housing estates</li> <li>• No bike lane as this would narrow MHR which needs to be widened</li> <li>• No need to reduce MHR to 40kmh</li> <li>• Concerned about loss of gum trees on MHR if development occurs</li> </ul>
34	29 April 2020	<ul style="list-style-type: none"> <li>• Agrees with sky rail type development with parking underneath and access through</li> <li>• MHR should be 4 lanes</li> <li>• No housing on railway parking area</li> <li>• Not happy with commuters being mixed up with playgrounds</li> </ul>
35	29 April 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• MHR already very narrow and congested</li> <li>• Large increase in density will have impact on amenity</li> <li>• Train frequency is going to increase but no consideration to traffic impacts</li> </ul>
36	30 April 2020	<ul style="list-style-type: none"> <li>• Like to see library incorporated into Diamond Creek leisure buildings</li> <li>• Unique semi-rural/eco- sustainable bent of the area expanded in style and development</li> <li>• Concerned about higher density – nothing over 3 levels</li> <li>• Does support extend hours of operation – some concerns over what types of business may develop – looking for artisan family business rather than franchise</li> <li>• Supports the idea of investigating additional potential uses in Precinct 3 and 4 but lots of consultation necessary</li> <li>• Likes the idea of employment anchor</li> </ul>

		<ul style="list-style-type: none"> <li>• Concerns over the sky rail</li> <li>• One way system in Elizabeth Street - good idea but should run in opposite direction</li> <li>• Likes ideas of better connections between and across activity centre</li> <li>• Supports MHR reduction to 40 kmh and to consider roundabout at Wensley Street/MHR</li> <li>• Raises some issues with regards to ease of use of some pedestrian treatments proposed</li> </ul>
37	30 April 2020 <b>Diamond Creek Traders Association</b>	<ul style="list-style-type: none"> <li>• The DC Traders Association highlight the need to undertake a much greater degree of consultation with businesses and the community on most of the proposals in the plan</li> <li>• Supportive of possible changes to the ACZ 2 to provide a wider possible range of uses</li> <li>• Support indented parking on Elizabeth Street</li> <li>• Support strengthening landscaping character</li> <li>• Keen to see a 'plaza' or 'heart of Diamond Creek' – important Diamond Creek doesn't just become a thoroughfare</li> <li>• Support some development in the area but concerned that the scope and scale will impact on Diamond Creek character and associated increase in traffic congestion</li> <li>• Qualified support for sky rail – based on long term goals</li> <li>• Community consultation included in the plan highlighted the people don't feel connected to the centres with ease of movement and the amenity being raised – this needs to be addressed</li> <li>• Don't support the idea of one way system in Elizabeth Street</li> <li>• Don't support the suggestion of 40kmh on MHR</li> <li>•</li> </ul>
38	29 April 2020	<ul style="list-style-type: none"> <li>• Wants AC boundary extended to include Diamond Street, Bage Street and Collins Street</li> <li>• Remove all aspects of plan that refer to Chute Street as a secondary node</li> <li>• Requests Precinct 1 be renamed and takes function of retail core no 1 with Precinct 4 being retail core 2</li> <li>• Wants to encourage maximum development including residential in Precinct 1</li> <li>• Suggest including bus transportation from Diamond Street, Bage Street and Collins Street, to include Chute Street to Train Station. Add new traffic lights and Pedestrian crossing at ground level at the Corner of Chute Street and Diamond Street and from the Train Station crossing over to Chute Street for safety</li> </ul>
39	30 April 2020	<ul style="list-style-type: none"> <li>• Believes Chute Street has been demoted from what should be prime location</li> <li>• Requests structure plan be disbanded – inequity to long standing investors with lands to north of activity centre</li> </ul>

		<ul style="list-style-type: none"> <li>• Requests land outside UGB be rezoned from RCZ to GRZ to allow subdivision rather than multi-level development (shoeboxes) in Diamond Creek</li> <li>• Argues that infrastructure exists in proximity to Diamond Creek (on their lands) in the Green Wedge land and that this should be used before need to create additional infrastructure</li> </ul>
40	01 May 2020 <b>Friends of Nillumbik</b>	<ul style="list-style-type: none"> <li>• Generally very supportive of structure plan</li> <li>• Any development should be high quality and sustainable</li> <li>• Agree with strengthening landscape character and set back options – prefers native options</li> <li>• Notes similarity of vision for both MAC's – each should be more individual</li> <li>• Supportive of the notion to increase the range of uses in Precincts 3 and 5 by adapting the ACZ2</li> <li>• Retail on both sides of MHR should continue to be supported and activated to ensure its long-term future.</li> <li>• Relocating Shire Office to Diamond Creek would not lead to an increase in local jobs but would boost activity for retail core and proximity to public transport means that fewer staff would need to drive</li> <li>• Suggests the location in Precinct 5 could be better used for a community hub - leaving the existing Community Building for leisure functions</li> <li>• Supportive of the objectives to cluster community facilities in the Activity Centre, focus retail activity in Precinct 4 alongside the Chute St group of shops in Precinct 1, and provide additional housing within the Activity Centre predominantly in Precincts 4 and 6.</li> <li>• In Precinct 4 in relation to TOD - clarity regarding train station commuter parking required</li> <li>• Concern over proposed height levels for development - suggestion that up to 5 levels in Precinct 3 could be seen as placing an endpoint on the existing businesses occupying the precinct.</li> <li>• Supportive of parking either at rear or basement of development and comments on poor pedestrian environment in Precinct 4.</li> <li>• Supportive of the identified need for improvements to footpaths, pedestrian crossings, on road cycle lanes, bike parking, and generally better connectivity throughout the Activity Centre</li> <li>• 40 kmh on MHR supported</li> <li>• Measures to improve pedestrian movement across and within the centre supported</li> <li>• Agree level crossing removal is needed but not through use of sky rail</li> <li>• Applauds the focus on the need for a 'public plaza', a community gathering space for Diamond Creek and agree Precinct 4 is the most logical location</li> </ul>

		<ul style="list-style-type: none"> <li>• Agrees with the identification that an ‘obvious deficit’ in the Activity Centre is a multi-purpose community building but states that Council must undertake significant consultation on both the functions and location of this potential building/hub</li> <li>• Advocacy for improvements at the Diamond Creek train station is important, including platform upgrades, accessibility, and the general look of the station.</li> <li>• Precinct 6 does not need the proposed level of development</li> </ul>
41	01 May 2020	<ul style="list-style-type: none"> <li>• <b>Gregg Street resident</b></li> <li>• Against any development above 2 levels in Precinct 5 and 6</li> <li>• Existing road congestion bad on MHR and development would make it worse – traffic would be pushed on to side roads</li> <li>• Will increase noise, impact privacy of residents</li> <li>• Diamond Creek’s attraction is its country feel/atmosphere and a quiet township. Think about the name of our shire - GREEN WEDGE SHIRE. Stand true to this name and its meaning.</li> </ul>
42	<b>Department of Transport</b>	<ul style="list-style-type: none"> <li>• DOT is seeking clarity on a number of issues with regard to Figure 7 (Movement and Access Framework Plan page 29) being: <ul style="list-style-type: none"> <li>○ proposal for a raised pedestrian crossing on Hurstbridge Road (stating proposed treatment is inconsistent with the function of an arterial road);</li> <li>○ Introduction of flashing lights (seeking clarification on treatment proposed); and</li> <li>○ On Road Bicycle Lanes (further clarification and consultation required).</li> </ul> </li> <li>• Suggestion to introduce a new figure to show, all existing pedestrian crossings and signals overlaid with the Structure Plan’s proposed transport initiatives so these transport elements are not considered in isolation of one another.</li> </ul>
43	<b>Melbourne Water</b>	<ul style="list-style-type: none"> <li>• States that there are high flood areas within the activity centre and these should be considered in support of the objective of Clause 13.03-1S to avoid intensifying the impact of flooding through inappropriately located use and development.</li> <li>• Melbourne Water recommends that the structure plan outline the areas of high flood hazard and where possible constrain or direct development (at permit stages) accordingly, or require flood mitigation measures.</li> <li>• Welcomes suggestions to improve and enhance the management and vegetation along the creek and states support for improving trail connectivity along the creek.</li> <li>• Suggests the following additional objectives for Precinct 2:</li> </ul>

		<ul style="list-style-type: none"> <li>○ <i>Continue to enhance the quality of the Diamond Creek.</i></li> <li>○ <i>Undertake additional planting in the parkland adjacent to the Diamond Creek.</i></li> <li>● States stormwater management should be considered further as part of the plan. For example, to reduce the extent to which the activity centre is a source of litter in the creek.</li> <li>● States Melbourne Water is willing to work with Council to improve the local waterways.</li> </ul>
44	29 May 20	<ul style="list-style-type: none"> <li>● Precinct One (Chute St) 5.1 of draft is the original historic Township. Precinct One needs to be renamed as Retail Core 1, to reflect its true position at the top of the town centre hierarchy. We therefore strenuously object to both the new title as 'Chute St Commercial Strip' and its connected objective as 'retail, office, entertainment activities and as a Secondary node to the retail core in Precinct 4'. We ask for these objectives to be removed.</li> <li>● We request the Activity Centre be extended to include the original township of Diamond Creek including Diamond St, Bage St and Collins St, the Primary School, the original Police Station and the original Church on Chute Street.</li> <li>● We object to the new inclusion of the Creek side open space area now called Precinct 2 and schools and recreation now called Precinct 7 within the Activity Centre Zones. This will alter the strategic growth policies and strategies in and around the centre.</li> <li>● All investment needs to be encouraged to make use of Precinct One before any incentives are offered for going elsewhere.</li> <li>● We consider it important to preserve the character of Diamond Creek and therefore suggest the building heights to be optional to three stories and no more.</li> <li>● It essential that traffic lights and a pedestrian crossing at ground level are installed on the corner of Diamond St and Chute St as this is a dangerous intersection with the potential for a serious accident. This will also allow traffic to safely turn from Chute St into Diamond St</li> <li>● There is the opportunity for an anchor business being located in this area rather than east of the railway line. This will balance business support in Precinct 1.</li> <li>● We object to Station St becoming a one way street as this will negatively impact on the entry and exit to Chute Street.</li> </ul>



**From:** [Engagement](#)  
**To:** [Strategic Planning](#)  
**Subject:** FW: Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 20 April 2020 1:53:46 PM

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**engagement**

**Nillumbik Shire Council**

[engagement@nillumbik.vic.gov.au](mailto:engagement@nillumbik.vic.gov.au)

*Nillumbik Shire Council acknowledges and pays respect to the Wurundjeri people, the traditional custodians of these lands and waters.*

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**From:** no-reply@harvestdp.com <no-reply@harvestdp.com>  
**Sent:** Monday, 16 March 2020 10:55 AM  
**To:** Engagement <engagement@nillumbik.vic.gov.au>  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

**Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

Overall supportive of changes that enhances the semi-rural/heritage feel of the town centre.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

*[Theme 1: Land Use Activities and Development](#)*

### **Do you have any comments about Theme 1?**

Any development for additional higher density housing in the centre to meet projected needs, should be restricted to a human form with a limit on the height of the apartments above street level. 5 stories does not fit in well with the semi rural setting of Diamond Creek. Also introduction of 300+ apartments should come with caveats that do not impinge upon the trading of the hospitality and industrial uses in the area.

### **Theme 2: Built Form Character**

#### **Do you have any comments about Theme 2?**

The proposed increased building heights of no greater than 5 storeys, is contrary to the semi rural feel of the town centre and should be retain existing height controls if not lowered Will car parking either provided in basements in zone 4 be below the flood plain and should the plan recognise and acknowledge this? Built form adjacent the Diamond Creek Tavern on approaches and from the west side of Coventry oval should ensure it does not dominate the heritage form of the tavern's roof line. The maximum height limit right next to the tavern is way to high.

### **Theme 3: Movement and Access**

#### **Do you have any comments about Theme 3?**

Nothing in the plan has indicated a long community recognition that traffic congestion, in the small morning and afternoon peaks, is in part due to the to the level crossing but also, because on both sides of Main Hurstbridge Road goes from 2 lanes to one. Congestion could be alleviated in leveraging off the *"existing"* road reserve and duplicating Main road throughout its length. This may mean that a more costly Skyrail solution is not required. The plan should capitalise on the road reserve and create a country style wide avenue through the town centre. The community does not understand how Skyrail would dominate the skyline through the town centre and create a visual divide between the the north west and south east parts of the centre that would destroy once and for all the semi-rural look and feel of the town centre. Furthermore it is false to suggest that creation of public open spaces below Skyrail would create a welcoming environment, rather it would be subject to overshadowing and appear brutal Stalinist in outlook with mass of concrete above ones head. This plan for Skyrail should be socialised a lot more before it makes way into the plan. Underpasses, as proposed for Main Hurstbridge Road should be eliminated from the plan, as they are subject to undesirable anti-social behaviour and pedestrians would feel unsafe using it. With the reduction of the speed limit to 40kmh through the area and possible shared zone areas, eg see the link between Ringwood railways station and Eastland, a more inviting way to ensure the safety of pedestrians can be achieved.

### **Theme 4: Landscape and Place Activation**

#### **Do you have any comments about Theme 4?**

The opportunity for a public space/ plaza and the 'heart' of the Activity Centre, would be ideally situated in the corner wedge car-park area in front of Coles *\*right now\**. This area could be developed for outdoor dining and have a small amphitheatre for buskers and other community events. Having the plaza located near the train station would be



placing it too far from the activation areas of the town. The public space should not be overshadowed by high structures, so that users do not feel closed in, with plenty of natural sunlight access. From this public space there should be better access to the train station through the walkway that is between Coles and the real estate agent. Currently there is a chain link fence and users need to walk the long way around to access the train station.

### *Theme 5: Community and Leisure*

#### **Do you have any comments on Theme 5?**

I would suggest that the Senior Citizens Building be redeveloped to meet the needs for a larger community hall and other meeting spaces that is suitable for diverse community needs that is close to the train station.

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### *Precinct 1: Chute Street Commercial Strip*

#### **Do you have any comments on Precinct 1?**

A 3 story limit through out the area should be the maximum. A 4 story limit on Hyde Street would not only over shadow the residential homes, nut also dominate the visual tree landscape height.

### *Precinct 2: Creek-side Open Spaces*

#### **Do you have any comments on Precinct 2?**

If an underpass is required under Main Hurst bridge road, then look at placing it under the DC bridge. next to the abutments.

### *Precinct 3: Elizabeth Street / Station Street*

#### **Do you have any comments about Precinct 3?**

Limit the height of buildings surrounding the DC Tavern so as not to dominate its roof line.

### *Precinct 4: Retail Core*

#### **Do you have any comments about Precinct 4?**

Do not include the Skyrail concept. until other options like duplication of Main Hurstbridge Road is investigated and exhausted. Do not increase the current overall height limit from 3/4 to 5 stories as it would result in the destruction of the semi rural feel of the town centre. Furthermore, there has been no appetite to have built to the current 3 story limit, so the increase is not necessary.

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

**Do you have any comments about Precinct 5?**

No comment.

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

**Do you have any comments about Precinct 6?**

No comment.

*Precinct 7: Schools and Recreation*

**Do you have any comments about Precinct 7?**

No comment.

**Do you have any other comments about the draft Structure Plan?**

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**Subject:** FW: Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 20 April 2020 1:53:54 PM

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com) <[no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)>  
**Sent:** Saturday, 14 March 2020 3:33 PM  
**To:** Engagement <[engagement@nillumbik.vic.gov.au](mailto:engagement@nillumbik.vic.gov.au)>  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

**Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

Thank you for all the hard work that has gone into this plan.

### Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

*[Theme 1: Land Use Activities and Development](#)*

**Do you have any comments about Theme 1?**

*[Theme 2: Built Form Character](#)*

**Do you have any comments about Theme 2?**

*[Theme 3: Movement and Access](#)*

**Do you have any comments about Theme 3?**

Most of this is great but could you consider:

1. Moving the pedestrian crossing in Chute Street to the end of Diamond Street to better



connect inline with the path the comes out near the loos. This would also help with the traffic congestion on netball days. It is virtually impossible to turn right out of Diamond Street on Saturdays until after 3pm. Also people run across there a lot. It's an obvious crossing point.

2. There really does not need to be an over pass at the end of Diamond St there if you move the lights to that point.

3. Re-open Hyde Street to allow another way for cars to flow.

4. It is really hard to cross the rail line and then the weird little intersection at Fyffe and Brooks st. I know it's a VicRoads road and the state should really pay for it. The path across the train line there needs to be much wider and maybe a zebra crossing over the side streets could help.

### *Theme 4: Landscape and Place Activation*

#### **Do you have any comments about Theme 4?**

Council has done some AMAZING treatments in roundabouts in Nillumbik so I look forward to seeing more of that.

Please look at the health of the River Red Gum in the carpark in Diamond Street, if that car park is going to be re-zoned, that tree could be threatened and it a beautiful landmark.

It would be great to see how additional planting could be used to buffer the cafes from the traffic. It is not the best sitting outside the cafes in Chute street because the traffic noise is so bad. Could a few carparks be taken out and planted up?

Also, on Wattle Day the steam train runs through this space. Why not plant wattles that flower in that month and make a bigger tourism fuss about it. A. paradoxa would do it, and the cost of planting a couple hundred along the edge of the track would not be major. Tourism is an industry that should be pushed harder and parklands provide a good draw card.

### *Theme 5: Community and Leisure*

#### **Do you have any comments on Theme 5?**

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### *Precinct 1: Chute Street Commercial Strip*

#### **Do you have any comments on Precinct 1?**



Good idea to increase the heights and encourage mixed use buildings. If the Hyde Street frontage is going to be included then Hyde Street should really be re-opened to allow cars to access main road.

*Precinct 2: Creek-side Open Spaces*

**Do you have any comments on Precinct 2?**

Behind the tennis courts there is an old farm dam. Once I have seen it full and it looked amazing. Could a diversion off the courts or Challenger street be used to fill it with run off? This could support tourism.

*Precinct 3: Elizabeth Street / Station Street*

**Do you have any comments about Precinct 3?**

*Precinct 4: Retail Core*

**Do you have any comments about Precinct 4?**

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

**Do you have any comments about Precinct 5?**

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

**Do you have any comments about Precinct 6?**

*Precinct 7: Schools and Recreation*

**Do you have any comments about Precinct 7?**

**Do you have any other comments about the draft Structure Plan?**

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**To:** [Strategic Planning](#)  
**Subject:** FW: Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 20 April 2020 2:13:09 PM

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com) <[no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)>  
**Sent:** Sunday, 22 March 2020 2:09 PM  
**To:** Engagement <[engagement@nillumbik.vic.gov.au](mailto:engagement@nillumbik.vic.gov.au)>  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

Having read the draft plan and in particular the proposed building height changes, has it not occurred to you to ask why the area's currently zoned 3 story areas, have only a handful of 2 story commercial buildings and a number of residential 2 story buildings? No one has to date utilised the 3 story's allowed. So what is the point of further increasing this? We neither want nor need a Doncaster like build up of the shopping precinct, indeed what we want is for Nillumbik Council to allow Diamond Creek to maintain its sleepy semi rural character. That is why we and many other professional and trade people chose to live here. I do agree the development of a central plaza may be beneficial, but see no real need to increase the evening use of the area.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *Theme 1: Land Use Activities and Development*

#### **Do you have any comments about Theme 1?**

Yes see above. We do not want (and will protest strongly against) further degradation of

our environment and personal safety inherent in higher density residential development or indeed any further mass housing estate development. Diamond Creek does not need to become another Doncaster or Mernda.

### *Theme 2: Built Form Character*

**Do you have any comments about Theme 2?**

### *Theme 3: Movement and Access*

**Do you have any comments about Theme 3?**

There is a need for duplication of the railway line and removal (over or under) of the level crossing.

### *Theme 4: Landscape and Place Activation*

**Do you have any comments about Theme 4?**

### *Theme 5: Community and Leisure*

**Do you have any comments on Theme 5?**

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### *Precinct 1: Chute Street Commercial Strip*

**Do you have any comments on Precinct 1?**

### *Precinct 2: Creek-side Open Spaces*

**Do you have any comments on Precinct 2?**

### *Precinct 3: Elizabeth Street / Station Street*

**Do you have any comments about Precinct 3?**

### *Precinct 4: Retail Core*

**Do you have any comments about Precinct 4?**

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

**Do you have any comments about Precinct 5?**

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

**Do you have any comments about Precinct 6?**

*Precinct 7: Schools and Recreation*

**Do you have any comments about Precinct 7?**

**Do you have any other comments about the draft Structure Plan?**

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Wednesday, 22 April 2020 7:18:12 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

Building any residential apartments here will be detrimental to the current residents. The roads will not cope with this. Our area has a semi rural appeal and 3 storey apartments will change our area for the worst. A revamp with more commercial spaces and a more modern feel, as well as removing the level crossing will be great. However the area does not have an apartment “feel” and the current residents love Diamond Creek the way it is. It will lower our house values due to congestion. The roads already struggle in peak hour as it is. You only have to look at areas like Eltham and Doreen. I also don't like the look of removing current residential homes on the IGA side. Again, a revamp of the shops and removal of the level crossing will be great however this plan is too much. We are not as busy as Eltham and would love to keep our semi rural environment while also creating more commercial and modern spaces. This plan does not achieve that.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

### Theme 2: Built Form Character

Do you have any comments about Theme 2?

### Theme 3: Movement and Access

Do you have any comments about Theme 3?

### Theme 4: Landscape and Place Activation

Do you have any comments about Theme 4?

### Theme 5: Community and Leisure

Do you have any comments on Theme 5?

## **Precincts**

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*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### Precinct 1: Chute Street Commercial Strip

Do you have any comments on Precinct 1?

### Precinct 2: Creek-side Open Spaces

Do you have any comments on Precinct 2?

### Precinct 3: Elizabeth Street / Station Street

Do you have any comments about Precinct 3?

### Precinct 4: Retail Core

Do you have any comments about Precinct 4?



*Precinct 5: East of George Street, North of Main Hurstbridge Road*

**Do you have any comments about Precinct 5?**

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

**Do you have any comments about Precinct 6?**

*Precinct 7: Schools and Recreation*

**Do you have any comments about Precinct 7?**

**Do you have any other comments about the draft Structure Plan?**

Building any residential apartments here will be detrimental to the current residents. The roads will not cope with this. Our area has a semi rural appeal and 3 storey apartments will change our area for the worst. A revamp with more commercial spaces and a more modern feel, as well as removing the level crossing will be great. However the area does not have an apartment "feel" and the current residents love Diamond Creek the way it is. It will lower our house values due to congestion. The roads already struggle in peak hour as it is. You only have to look at areas like Eltham and Doreen. I also don't like the look of removing current residential homes on the IGA side. Again, a revamp of the shops and removal of the level crossing will be great however this plan is too much. We are not as busy as Eltham and would love to keep our semi rural environment while also creating more commercial and modern spaces. This plan does not achieve that.

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Thursday, 23 April 2020 9:24:25 AM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

I have a few concerns about the new development plan regarding traffic flow, increase in trains in and out of diamond creek station and the height of new housing developments. If the trains increase their frequency their will need the level crossing to be removed prior to allow traffic to flow through an already contested main road and shopping area. Parking for residence of new builds would need to be included in any plans as there is no parking on main rd and increased traffic through small side streets and back roads would be unsafe and dramatically decrease liveability of our area.

I am also concerned about privacy for locals that will have a shared boundary with multi storey apartment buildings over looking their backyards etc.

Thank you to have considered my feedback.

Best regards Joshua Smyth

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes.*

*Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

### Theme 2: Built Form Character

Do you have any comments about Theme 2?

### Theme 3: Movement and Access

Do you have any comments about Theme 3?

### Theme 4: Landscape and Place Activation

Do you have any comments about Theme 4?

### Theme 5: Community and Leisure

Do you have any comments on Theme 5?

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### Precinct 1: Chute Street Commercial Strip

Do you have any comments on Precinct 1?

### Precinct 2: Creek-side Open Spaces

Do you have any comments on Precinct 2?

### Precinct 3: Elizabeth Street / Station Street

Do you have any comments about Precinct 3?

### Precinct 4: Retail Core

Do you have any comments about Precinct 4?



*Precinct 5: East of George Street, North of Main Hurstbridge Road*

Do you have any comments about Precinct 5?

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

Do you have any comments about Precinct 6?

*Precinct 7: Schools and Recreation*

Do you have any comments about Precinct 7?

Do you have any other comments about the draft Structure Plan?

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Saturday, 25 April 2020 8:23:16 AM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

**Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

### Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

#### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

g.

#### *Theme 2: Built Form Character*

**Do you have any comments about Theme 2?**

Five-storey buildings would also be an absolute eye-sore and create an over-crowded feeling. The only businesses that seem to survive in Diamond Creek are predominantly food outlets and beauty services. Are we going to have five level fast food outlets and beauty service buildings?

#### *Theme 3: Movement and Access*

**Do you have any comments about Theme 3?**

Absolutely do not want to see Skyrail introduced into Diamond Creek - if the rail-line can be lowered below the road surface this would be so much more appealing. I believe Skyrail would be an absolute eye-sore. I do not agree with the 40km speed limit along Main Diamond Creek Road. All crossings are traffic light activated - apart from the School Crossing at Diamond Creek East in which the speed is reduced at school times.

*Theme 4: Landscape and Place Activation*

**Do you have any comments about Theme 4?**

*Theme 5: Community and Leisure*

**Do you have any comments on Theme 5?**

I totally love the idea of a library at the Diamond Creek Community Centre - this is long overdue for Diamond Creek. This would have to be one of the most positive outcomes of this review.

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

*Precinct 1: Chute Street Commercial Strip*

**Do you have any comments on Precinct 1?**

*Precinct 2: Creek-side Open Spaces*

**Do you have any comments on Precinct 2?**

*Precinct 3: Elizabeth Street / Station Street*

**Do you have any comments about Precinct 3?**

If Station Street traffic is made to travel one-way, this would also have to apply with Elizabeth Street - currently the onstreet parking makes it very dangerous to travel along.



*Precinct 4: Retail Core*

Do you have any comments about Precinct 4?

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

Do you have any comments about Precinct 5?

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

Do you have any comments about Precinct 6?

*Precinct 7: Schools and Recreation*

Do you have any comments about Precinct 7?

Do you have any other comments about the draft Structure Plan?

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** Strategic Planning  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Sunday, 26 April 2020 4:07:16 PM

---

## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

I am absolutely opposed to any all areas where multi-story buildings are proposed. It all areas suggested these will create increased traffic in areas already struggling to cope with traffic and parking. In all areas especially section 6 on Main Hurstbridge Road multi-story buildings will dramatically impact the views and outlooks of existing residents. We all live in this town for the community feel and the trees, and landscape views. It's why we choose to live in the green wedge! If you allow such industrialized development you are stripping us of the very reasons we choose to live her in the first place. I absolutely can not support this proposal. I do support improvements but not to the detriment of my view and quiet living. I will not comment in each themed area as my opposition is to all areas.

## Themes

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### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

*Theme 2: Built Form Character*

**Do you have any comments about Theme 2?**

*Theme 3: Movement and Access*

**Do you have any comments about Theme 3?**

*Theme 4: Landscape and Place Activation*

**Do you have any comments about Theme 4?**

*Theme 5: Community and Leisure*

**Do you have any comments on Theme 5?**

## **Precincts**

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*Precinct 1: Chute Street Commercial Strip*

**Do you have any comments on Precinct 1?**

*Precinct 2: Creek-side Open Spaces*

**Do you have any comments on Precinct 2?**

*Precinct 3: Elizabeth Street / Station Street*

**Do you have any comments about Precinct 3?**

*Precinct 4: Retail Core*

**Do you have any comments about Precinct 4?**

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

**Do you have any comments about Precinct 5?**

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

**Do you have any comments about Precinct 6?**

*Precinct 7: Schools and Recreation*

**Do you have any comments about Precinct 7?**

**Do you have any other comments about the draft Structure Plan?**

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 27 April 2020 10:29:23 AM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

**Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

I think it is wrong-headed in trying to emulate Greensborough rather than Hurstbridge. We do not want five storey buildings and sky rail - we are supposed to be a semi-rural town with green space and a homely feel.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *[Theme 1: Land Use Activities and Development](#)*

**Do you have any comments about Theme 1?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

### *[Theme 2: Built Form Character](#)*

**Do you have any comments about Theme 2?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

### Theme 3: Movement and Access

#### **Do you have any comments about Theme 3?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

### Theme 4: Landscape and Place Activation

#### **Do you have any comments about Theme 4?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

### Theme 5: Community and Leisure

#### **Do you have any comments on Theme 5?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

## **Precincts**

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*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### Precinct 1: Chute Street Commercial Strip

#### **Do you have any comments on Precinct 1?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

### Precinct 2: Creek-side Open Spaces

#### **Do you have any comments on Precinct 2?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

### Precinct 3: Elizabeth Street / Station Street



**Do you have any comments about Precinct 3?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

*Precinct 4: Retail Core*

**Do you have any comments about Precinct 4?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

**Do you have any comments about Precinct 5?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

**Do you have any comments about Precinct 6?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

*Precinct 7: Schools and Recreation*

**Do you have any comments about Precinct 7?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

**Do you have any other comments about the draft Structure Plan?**

No five storey buildings, no sky rail. We have no need for removing the level crossing as the bottleneck is caused by the narrowing road, not the crossing.

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 27 April 2020 1:22:17 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

I have major concerns with this Draft that has been put forward. There are countless contradictions of wanting to keep the rural community feel of the town but than talk of adding medium density, multi storey dwellings - "Objective: To retain the semi rural township character of Diamond Creek by clustering new development and activity in existing key locations..." PG.23 "To ensure development retains and enhances the townships character with medium scale buildings..." PG.23

This report even highlights the fact that there is "community opposition" PG.19 for additional dwellings in town, so how is it possible that this report is primarily focusing on the development of 303 dwellings over multiple storeys?

The report also highlights the fact that Diamond Creek has an aging population and tries to justify this statistic with urban development - counter point; how are these aging men and women meant to get to the shops to buy their groceries from Diamond Creek when you are advocating "...to reduce car dependency within the community by developing a network of local and regional shared paths for recreational and functional use.." PG.27 for car parking either in basements or behind the building?PG24 Also clearly stating that these people do currently drive to the town centre at the moment PG.26. I have a number of neighbours that fit this demographic on my street and I can assure you they have no interest in living in an apartment building.

Myself and many, many others that I have spoken to in regards to this plan are strongly opposed to it. Diamond Creek is a well loved town with a very strong community feel to it, the majority of this proposal is a mockery of what Diamond Creek stands for. The people of Diamond Creek are open to new development and business in the local area but this is to be done in a respectful way that compliments the town. In case this isn't abundantly clear yet - that does not mean up to 5 storey buildings surrounding the town!



Focuses on upgrades to the town centre and potentially a second storey may gain community support. Focus on another lane each way through the town to ease congestion may get support, and also advocating for greater public transport.

## Themes

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### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

### *Theme 2: Built Form Character*

**Do you have any comments about Theme 2?**

### *Theme 3: Movement and Access*

**Do you have any comments about Theme 3?**

### *Theme 4: Landscape and Place Activation*

**Do you have any comments about Theme 4?**

### *Theme 5: Community and Leisure*

**Do you have any comments on Theme 5?**

## Precincts

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full*

*details, see the draft plan.*

### *Precinct 1: Chute Street Commercial Strip*

#### **Do you have any comments on Precinct 1?**

Strongly oppose. This is a fantastic shopping hub which will be ruined by 3-4 storey development with additional dwellings. This will also directly and adversely affect current residential properties along Hyde Street which will also have a major impact on overshadowing given their locations. An increased emphasis on local business is great, but once again within the towns parameters. Also, no vertical tall continuous wall through this area, this is a beautiful and open town - it doesn't need to be changed to feel caged.

I disagree on the need for a raised walking track over Diamond Street, I believe it needs to be made safer than it currently stands but this is achievable without adding a visual eyesore to the area.

### *Precinct 2: Creek-side Open Spaces*

#### **Do you have any comments on Precinct 2?**

Love the emphasis on improved lighting, footpaths, play and recreational activities! These are all qualities that compliment the town and should be complimenting the focus of a NON multi storey town centre.

### *Precinct 3: Elizabeth Street / Station Street*

#### **Do you have any comments about Precinct 3?**

Strongly oppose.

An absurd proposal to want 4-5 storey buildings in such an area. This would over shadow the Diamond Creek Tavern (a heritage building) as well as give a horrible visual entrance to the town. How the plan can try justify saying this is a semi industrial area and than want to place dwellings above it is also of concern.

Further to that, having additional people living in the area and having NO plans for upgrades to the roads is an all too often oversight that is completely unacceptable! The vast majority of people will require a car - this pathetic addition of parking to the side of Elizabeth street will not even come close to what is required. Especially as plans suggest for parking in the retail centre to be removed will leave the town massively under capacity.

### *Precinct 4: Retail Core*

#### **Do you have any comments about Precinct 4?**

Strongly oppose.

This is not Greensborough, nor is it Heidleberg or Box Hill. A 4-5 storey plaza, office



spaces and apartments is completely off par with community feel and expectations. A greater emphasis on more local business's and variety is great - but not a scale this large. Diamond Creek is a very community focused town that support their local business's. Variety is great and restaurants are widely accepted, a new development has a lot of potential to enhance this but it needs to be reflective of the area. Can't say it enough times - additional lanes are an absolute must through main Hurstbridge road! The draft even clearly states this area is "some of the States most bushfire vulnerable land" PG.27 and yet it has the audacity to suggest an additional 303 dwellings and no such upgrades to the main thoroughfare that is the main access and egress point for this and other surrounding towns!? Can the council stand in front of the coroner and justify that people were meant to escape a bushfire on their bikes?

### *Precinct 5: East of George Street, North of Main Hurstbridge Road*

#### **Do you have any comments about Precinct 5?**

Strongly oppose.

Again, absolutely Ludacris to want to place 4-5 storey buildings throughout this area. Can I ask why the council might see it fit to spend a large sum of money to build a new Civic building? After all, this would be paid out of residents rates so what is the justification that this would be a requirement?

Further to that, this area incorporates critical emergency services such as the Diamond Creek VicPol branch and CFA Volunteers. Having multi storey buildings placed through here in the attempt to have a higher number of rate payers is an insult to all of these emergency service workers / volunteers and the greater community who rely on them. What kind of access and egress would be applied here? What would occur with the noise that is required from emergency service appliances when responding to an incident?

### *Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

#### **Do you have any comments about Precinct 6?**

Strongly oppose.

3-4 storey buildings along a strip that directly align with current residential areas is absurd. This would also effect all residents to the south of these buildings with a harsh view, privacy issues and overshadowing.

### *Precinct 7: Schools and Recreation*

#### **Do you have any comments about Precinct 7?**

#### **Do you have any other comments about the draft Structure Plan?**

Improvement and modernising is imperative - please do it in a manner that is reflective of the area and most importantly, upgrade the roads and infrastructure before any of this takes place.



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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 27 April 2020 2:47:17 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

The potential for five storeys in Diamond Creek is concerning. This would give it a feel like Greensborough. Although Greensborough can be considered as having a strong presence of trees and other vegetation, multiple storeys in the main street completely destroy its sense rural and bushland character - I feel the same thing would happen in Diamond Creek if more storeys were added to current buildings. The Community Hub needs to be a place that invites the public to enter. I have lived in Diamond Creek for a year and lived in the area since I was born - I only found out it was the Community Hub a week ago. Please consider more spaces open for the public to play sport. Diamond Creek East Primary is the only place to kick a soccer ball and practise shooting hoops for netball and basketball. This has been made evident during the coronavirus pandemic, with the courts full every night and limited opportunity to have space to enjoy. Opportunity to engage in this area is also limited during the school term. Please consider planting as many trees and shaded areas as possible. The images provided in the draft plan don't seem to have much vegetation. This will increase the urban heat island effect in Diamond Creek which is dangerous for health and wellbeing. I want to commend the council for its efforts in upgrading the public nature space down near the Rotary Park and netball courts. This space is beautiful for dog walkers, cyclists, runners and families - I have personally felt mental and physical health benefits from using this area.

## **Themes**

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes.*

*Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

### *Theme 2: Built Form Character*

**Do you have any comments about Theme 2?**

### *Theme 3: Movement and Access*

**Do you have any comments about Theme 3?**

### *Theme 4: Landscape and Place Activation*

**Do you have any comments about Theme 4?**

### *Theme 5: Community and Leisure*

**Do you have any comments on Theme 5?**

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### *Precinct 1: Chute Street Commercial Strip*

**Do you have any comments on Precinct 1?**

### *Precinct 2: Creek-side Open Spaces*

**Do you have any comments on Precinct 2?**

*Precinct 3: Elizabeth Street / Station Street*

Do you have any comments about Precinct 3?

*Precinct 4: Retail Core*

Do you have any comments about Precinct 4?

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

Do you have any comments about Precinct 5?

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

Do you have any comments about Precinct 6?

*Precinct 7: Schools and Recreation*

Do you have any comments about Precinct 7?

Do you have any other comments about the draft Structure Plan?

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 27 April 2020 2:54:16 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

**Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

I think such dense construction will completely ruin the rural ambience of Diamond Creek, plus, so much extra construction and no increase in road capacity would seem to be lunacy. I currently live approx. 750m from the shopping area and, on a Saturday morning, the weight of traffic means that a trip can take over an hour and a half to complete in a car. I feel very strongly that such a huge increase in construction would be entirely wrong

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *[Theme 1: Land Use Activities and Development](#)*

**Do you have any comments about Theme 1?**

### *[Theme 2: Built Form Character](#)*

**Do you have any comments about Theme 2?**



### Theme 3: Movement and Access

**Do you have any comments about Theme 3?**

Ridiculous to think of increasing building without increasing road access

### Theme 4: Landscape and Place Activation

**Do you have any comments about Theme 4?**

### Theme 5: Community and Leisure

**Do you have any comments on Theme 5?**

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### Precinct 1: Chute Street Commercial Strip

**Do you have any comments on Precinct 1?**

### Precinct 2: Creek-side Open Spaces

**Do you have any comments on Precinct 2?**

### Precinct 3: Elizabeth Street / Station Street

**Do you have any comments about Precinct 3?**

### Precinct 4: Retail Core

**Do you have any comments about Precinct 4?**

### Precinct 5: East of George Street, North of Main Hurstbridge Road

**Do you have any comments about Precinct 5?**

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

Do you have any comments about Precinct 6?

*Precinct 7: Schools and Recreation*

Do you have any comments about Precinct 7?

Do you have any other comments about the draft Structure Plan?

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 27 April 2020 8:10:17 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

As a new resident (and hopefully long term) of Diamond Creek, I am invested in its future and of course wish for it to be able to grow and prosper..... HOWEVER, what brought us here IS the fact that it's got a small country town feel, rather than being a Melbourne suburb.

My fear is that this plan will remove that feel and turn this wonderful little town into another bloody Melbourne suburb.

Seriously, 5 storey buildings? Are you kidding me? Just NO.

If this plan goes ahead it will kill what's good about DC.

I agree that the area needs a plan, an update and the roads need fixing to correct the epic traffic problems but that will all be for nothing when you get greedy and over develop it.

I don't have the time to give this a detailed point by point response but I have to just give my most important comments and it is all in line with what I have said.

In this day and age where money rules, councils are overstepping their bounds everywhere and its the residents that are left with the consequences of their grandiose "visions". Please don't do that to our Diamond Creek. By all means, update it and fix the bloody railway crossing but don't get greedy and overdevelop it, leave it alone!

# Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

## [Theme 1: Land Use Activities and Development](#)

Do you have any comments about Theme 1?

## [Theme 2: Built Form Character](#)

Do you have any comments about Theme 2?

## [Theme 3: Movement and Access](#)

Do you have any comments about Theme 3?

## [Theme 4: Landscape and Place Activation](#)

Do you have any comments about Theme 4?

## [Theme 5: Community and Leisure](#)

Do you have any comments on Theme 5?

# Precincts

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines. You can see the [full Precinct Plan here](#). Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

## [Precinct 1: Chute Street Commercial Strip](#)

Do you have any comments on Precinct 1?



Precinct 2: Creek-side Open Spaces

**Do you have any comments on Precinct 2?**

Precinct 3: Elizabeth Street / Station Street

**Do you have any comments about Precinct 3?**

5 storey buildings..... NO. This is not inner Melbourne, this is Diamond Creek, leave it as a village.

Precinct 4: Retail Core

**Do you have any comments about Precinct 4?**

Precinct 5: East of George Street, North of Main Hurstbridge Road

**Do you have any comments about Precinct 5?**

Precinct 6: East of Wensley Street, South of Main Hurstbridge Road

**Do you have any comments about Precinct 6?**

Allowing this to be developed with 4 storey apartment/office buildings here will remove the northern light from the Gregg St houses behind, increase parking, increase traffic problems in and around Gregg st and finally, it will kill the atmosphere of DC.

JUST NO. Don't overdevelop it! You're getting greedy guys!

Precinct 7: Schools and Recreation

**Do you have any comments about Precinct 7?**

**Do you have any other comments about the draft Structure Plan?**

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 27 April 2020 8:50:16 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

Yes - I have big concerns!

People live in Diamond Creek because they love the community feel and having space - we don't want to feel like we're living in the inner city suburbs!

Do not understand how there are plans for all these apartment buildings with no plans for widening the main road which is already an issue.

COVID-19 has shown us how lucky we are to have parks and open spaces - don't ruin it!

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *[Theme 1: Land Use Activities and Development](#)*

#### **Do you have any comments about Theme 1?**

Do not understand the plan to develop the shops to a 5 story level! So over the top and unnecessary! The shops should just be refurbished and remain one level to main the community feel.

### *[Theme 2: Built Form Character](#)*

**Do you have any comments about Theme 2?**

This would destroy Diamond Creek's character. Cannot believe that "the green wedge council" is planning on building 5 stories throughout the main areas of Diamond Creek! People live in Diamond Creek because they want to be around nature, not feel like they are in the city!

*Theme 3: Movement and Access*

**Do you have any comments about Theme 3?**

Yes! The road already needs to be widened and you plan on building all these apartments and taking away the shops Car parks and only adding minimal parking to Elizabeth Street?! Does not make any sense! People that live in Diamond Creek need to be able to go to the shops and have somewhere to park their car.

*Theme 4: Landscape and Place Activation*

**Do you have any comments about Theme 4?**

5 story is ridiculous! We are not in the city and we don't want to turn into Doncaster!

*Theme 5: Community and Leisure*

**Do you have any comments on Theme 5?**

This would destroy Diamond Creek's community vibe. It would become just another town that could be in the city/inner suburbs. COVID-19 has demonstrated how important it is for people to have their own space - now more than ever you should be appreciating how lucky we are and maintain our beautiful town - it is possible to refresh the shopping area without making it into 5 stories high. Very disappointed by this plan.

## **Precincts**

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*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

*Precinct 1: Chute Street Commercial Strip*

**Do you have any comments on Precinct 1?**

Already a busy road, do not understand how you are planning to add more shops and

add apartments to this area when it is already so busy and in need of an upgrade!

*Precinct 2: Creek-side Open Spaces*

**Do you have any comments on Precinct 2?**

*Precinct 3: Elizabeth Street / Station Street*

**Do you have any comments about Precinct 3?**

Do not understand how you think there will be enough parking??!

*Precinct 4: Retail Core*

**Do you have any comments about Precinct 4?**

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

**Do you have any comments about Precinct 5?**

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

**Do you have any comments about Precinct 6?**

*Precinct 7: Schools and Recreation*

**Do you have any comments about Precinct 7?**

**Do you have any other comments about the draft Structure Plan?**

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Monday, 27 April 2020 9:27:16 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

Please do not ruin Diamond Creek with 5 story buildings and a sky rail. People live out here because it is not built up. I moved away from inner suburbs because of the built up development, hassle and bustle and concrete everywhere look. I love that Diamond Creek is not built up, it is peaceful. What you plan will take this away. You will destroy our suburb. Keep it low level, not intrusive. Please don't turn Diamond Creek into Greensborough. No to 5 story buildings. No to apartments and multistory dwellings and retail. Keep Diamond Creek quiet, calm and peaceful. Maybe just widen Main Hurstbridge Road to 2 lanes to help with traffic. Can an rail crossing underpass happen? Yes a town square would be great, something we are lacking.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *Theme 1: Land Use Activities and Development*

#### **Do you have any comments about Theme 1?**

Sounds very congested and claustrophobic. I do NOT want to live in suburb that is built up with 5 story buildings and a sky rail. I moved to Diamond Creek to get away from built

up main streets. Why does DC have to have high density housing? Wont this cause more traffic congestion. Are you trying to ruin what little 'semi rural' township feeling that is left here?

### Theme 2: Built Form Character

#### **Do you have any comments about Theme 2?**

No thanks to 5 story buildings anywhere in Diamond Creek. Just NO, NO, NO. Buildings built to the front boundary will feel invasive. Just like 5 story buildings. Again not why people live in Diamond Creek. Total opposite. How can 'rural village character' be achieved when you want to build high density clusters? Rural Village character in my opinion would not include buildings 3-5 stories high, more low and spread out. I agree that paths need to be connected. Especially an overpass from Diamond Valley Secondary side of creek/railway to bike track. This has been needed for years and keeps getting mentioned but never acted on. Again NO to 5 story buildings in Diamond Creek.

### Theme 3: Movement and Access

#### **Do you have any comments about Theme 3?**

This is not keeping in with the semi rural landscape. I see that you are trying to create an over pass to help traffic but this is a bit much. The colour scheme is horrendous, not keeping in with heritage character. Is there no chance of an underpass?

### Theme 4: Landscape and Place Activation

#### **Do you have any comments about Theme 4?**

I agree with creating open spaces for recreation and bike path. What is happening with the extension to Hurstbridge?

### Theme 5: Community and Leisure

#### **Do you have any comments on Theme 5?**

It's great to see the redevelopment of sporting facilities. How about the much loved pool? That could do with some money thrown at it?

## **Precincts**

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*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*



*Precinct 1: Chute Street Commercial Strip*

Do you have any comments on Precinct 1?

*Precinct 2: Creek-side Open Spaces*

Do you have any comments on Precinct 2?

*Precinct 3: Elizabeth Street / Station Street*

Do you have any comments about Precinct 3?

*Precinct 4: Retail Core*

Do you have any comments about Precinct 4?

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

Do you have any comments about Precinct 5?

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

Do you have any comments about Precinct 6?

*Precinct 7: Schools and Recreation*

Do you have any comments about Precinct 7?

Do you have any other comments about the draft Structure Plan?

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Tuesday, 28 April 2020 11:48:16 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

**Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

### Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

#### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

I am not in favour of a transit oriented development that has residential above. A big NO to inappropriate 4 & 5 storey development. Diamond Creek needs to retain its charm. It should not be developed into a suburb devoid of character. I have been told that once there was a mine at Diamond Creek that was closed off & destroyed. Such a shame that this decision was made as it ignores the tourism potential & character retention that this could have had for Diamond Creek in its modern setting in Melbourne.

#### *Theme 2: Built Form Character*

**Do you have any comments about Theme 2?**

I oppose the development of 4 & 5 storey buildings in the Diamond Creek Activity

Centre.

### *Theme 3: Movement and Access*

#### **Do you have any comments about Theme 3?**

The possibility of a sky rail at Diamond Creek makes sense to allow for the removal of the Main-Hurstbridge Rd level crossing which causes much congestion.

The design images shown in this plan do not appeal to me as there is an overload of the built form. Much more landscaping & greening would be needed to make this a pleasant place to be. This is the heart Diamond Creek. It is not Rosanna. As such attention landscape & place activation details would be imperative.

### *Theme 4: Landscape and Place Activation*

#### **Do you have any comments about Theme 4?**

Diamond Creek underpass is a good idea to connect the trail.

### *Theme 5: Community and Leisure*

#### **Do you have any comments on Theme 5?**

Looks good.

## **Precincts**

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*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### *Precinct 1: Chute Street Commercial Strip*

**Do you have any comments on Precinct 1?**

### *Precinct 2: Creek-side Open Spaces*

**Do you have any comments on Precinct 2?**

### *Precinct 3: Elizabeth Street / Station Street*

Do you have any comments about Precinct 3?

*Precinct 4: Retail Core*

Do you have any comments about Precinct 4?

*Precinct 5: East of George Street, North of Main Hurstbridge Road*

Do you have any comments about Precinct 5?

*Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

Do you have any comments about Precinct 6?

*Precinct 7: Schools and Recreation*

Do you have any comments about Precinct 7?

Do you have any other comments about the draft Structure Plan?

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Wednesday, 29 April 2020 10:53:16 AM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

I think there are a lot of good ideas in there. Revitalising the retail area, making Station Street one-way, increasing pedestrian and bicycle accessibility and retaining the natural feel of the area are all excellent ideas. My main concerns relate to the visual pollution caused by 4 and 5 story buildings, as well as potential parking issues caused by increased residents and visitors to Diamond Creek.

## Themes

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### *Theme 1: Land Use Activities and Development*

#### **Do you have any comments about Theme 1?**

I think any vision of employment options in the area needs to be fleshed out, to avoid building commercial spaces that will not be used. Higher density housing will also alter the feel of the village, not necessarily for the good. I think more consideration needs to be given to the feel of the area: do we want a 'boutique', 'artisan' feel, or do we want to pack in as many people as we can.



## Theme 2: Built Form Character

### **Do you have any comments about Theme 2?**

Can we please get rid of the McDonald's or make it less conspicuous? Otherwise, I think the general feel of this theme is fine. If we can make car parks less obvious, while not losing capacity, that is a good thing.

## Theme 3: Movement and Access

### **Do you have any comments about Theme 3?**

Some great ideas, including the underpass connecting the Trail. Redesigning car parks, as long as no spaces are lost, is also a great idea. 40 km/h speed limit is fine as well. Removing the level crossing is a good idea, but a sky rail would look ugly and tarnish the existing vista. I believe further consideration should be given to any possible way of allowing pedestrian and bicycle thoroughfare under the train station (appreciating geological impediments). I like the Gipson Street bridge as it is: it works well and encourages motorists to be patient and polite (which they nearly always are). Any changes would impact the feel of this leafy and rustic area of the suburb.

## Theme 4: Landscape and Place Activation

### **Do you have any comments about Theme 4?**

References to local heritage can be done well (i.e. Hurstbridge) and done in a tacky manner (i.e. plenty of other suburbs). This should be done carefully and in a restrained manner. Retaining or increasing vegetation is a great idea, with the more natives the better. There is a lot of bird life in the area and this should be encouraged. A public plaza is a good idea, as are better links to parkland and additional railway crossings for pedestrians and cyclists.

## Theme 5: Community and Leisure

### **Do you have any comments on Theme 5?**

Improving and modernising the train station is a great idea that I fully support

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### Precinct 1: Chute Street Commercial Strip

#### **Do you have any comments on Precinct 1?**

Retaining the feel of this strip is a great idea. If parking could somehow be increased, or public transport improved that could generate more movement and visitation of the precinct

### Precinct 2: Creek-side Open Spaces

#### **Do you have any comments on Precinct 2?**

This sounds great, including the underpass and extended bike path.

### Precinct 3: Elizabeth Street / Station Street

#### **Do you have any comments about Precinct 3?**

Making Station Street one way is a good idea. I don't agree with duplicating the Gipson Street bridge as I don't think it is necessary. Duplication would see cars moving faster through that area. If we could remove McDonald's or at least do something to hide it that would be wonderful. As it is, it's embarrassing to have that so close to the Diamond Creek sign.

### Precinct 4: Retail Core

#### **Do you have any comments about Precinct 4?**

More retail is a good idea, as long as there is a sound plan in place and there won't just be a lot of empty offices. Much consideration will need to be given to traffic, as getting in and out of the car park is presently fraught with danger. Please retain trees for the populations of corellas and other birds dwelling in this area.

### Precinct 5: East of George Street, North of Main Hurstbridge Road

#### **Do you have any comments about Precinct 5?**

No additional comments. I think this precinct could really do with some work to revitalise it and improve access

### Precinct 6: East of Wensley Street, South of Main Hurstbridge Road

#### **Do you have any comments about Precinct 6?**

No additional comments - all sounds fine

### Precinct 7: Schools and Recreation

#### **Do you have any comments about Precinct 7?**

No additional comments - all sounds fine

**Do you have any other comments about the draft Structure Plan?**

It's a great thing to be considering. I am all for improvement and change, while retaining the existing feel of the area and being conscious of transport and parking concerns.

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Wednesday, 29 April 2020 1:44:15 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

The vision seems like a cookie cutter for many other areas in increasing the retail landscape at the expense of what the area offers. The first couple of pages mention the rural and bushland character but 5 storey developments on the main street will not encourage or complement this. A tagline for Diamond Creek was where country meets city but this plan appears to be more city meets city and will make Diamond Creek look like central Greensborough.

The recent challenges in 2020 - bushfires and Covid-19 - have shown that both online and local shopping have increased so pushing a vision that will focus on chains and larger stores seems to be looking at what worked in the past. The local shops have been amazing in this challenging times and Diamond Creek should look to capitalise on the independent traders and aim to be more Daylesford/Healsville than inner city.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes.*

*Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

The employment anchor is a great idea and supporting boutique and artisanal food production is great.

Increased density is already happening in the area surrounding the shops ( Haley street and surrounds) with existing blocks being subdivided into smaller block sizes - 250-350m - which is similar to inner city areas. The area is in a higher risk bushfire zone so increasing accomdation with the limited road access will present difficutties for evacuations

### Theme 2: Built Form Character

#### **Do you have any comments about Theme 2?**

Retaining the semi rural character is admirable but how can that be completed with a 5 storey building?

### Theme 3: Movement and Access

#### **Do you have any comments about Theme 3?**

Until the level crossing is removed trainins cannot increase in numbers which restricts the needed improvements. Having trains offpeak every 20 mins and peak 10mins would change the focus. Buses whilst helpful for local transport get stuck in the traffic and cannot compete with more frequent trains.

The sky-rail is a challenge since it would dominate the area and improve the trains but significantly impact the semi rural character

### Theme 4: Landscape and Place Activation

#### **Do you have any comments about Theme 4?**

More public spaces in the central area are needed however the area is in a green wedge so working on connecting the existing green areas would be excellent

### Theme 5: Community and Leisure

#### **Do you have any comments on Theme 5?**

Community spaces and the upgrade of the Community centre would be welcomed and a library would be helpful for many reseidents of outlying communities as well as Diamond Creek .

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*



*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### *Precinct 1: Chute Street Commercial Strip*

**Do you have any comments on Precinct 1?**

### *Precinct 2: Creek-side Open Spaces*

**Do you have any comments on Precinct 2?**

### *Precinct 3: Elizabeth Street / Station Street*

**Do you have any comments about Precinct 3?**

### *Precinct 4: Retail Core*

**Do you have any comments about Precinct 4?**

The recent challenges in 2020 - bushfires and Covid-19 - have shown that both online and local shopping have increased so pushing a vision that will focus on chains and larger stores seems to be looking at what worked in the past. The local shops have been amazing in this challenging times and Diamond Creek should look to capitalise on the independent traders and aim to be more Daylesford/Healsville than inner city.

### *Precinct 5: East of George Street, North of Main Hurstbridge Road*

**Do you have any comments about Precinct 5?**

### *Precinct 6: East of Wensley Street, South of Main Hurstbridge Road*

**Do you have any comments about Precinct 6?**

Integrating into the area is great but how can that be done with 5 storey buildings in a mainly single storey setting

### *Precinct 7: Schools and Recreation*

**Do you have any comments about Precinct 7?**

Access to the area via the bridge over the train line is good but managing this with a large number of people walking across the school grounds will present challenges for student safety

**Do you have any other comments about the draft Structure Plan?**

The overall approach seems to be increasing retail space and increased density of housing. The world is changing and given how close we are to other retail spaces - Eltham, Greensborough, Plenty Valley - doesn't seem to be using the Green wedge area to its potential

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Wednesday, 29 April 2020 7:45:15 PM

---

## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

**Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

This plan is over the top, you can remove a level crossing (which we need) without giving yourself the authority to erect 5 story buildings. Diamond Creek should remain the size it is, it provides adequate facilities and service for for the size of the community, and these plans disrespect the nature of a rural shire such as Nillumbik.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *Theme 1: Land Use Activities and Development*

**Do you have any comments about Theme 1?**

### *Theme 2: Built Form Character*

**Do you have any comments about Theme 2?**

### *Theme 3: Movement and Access*

Do you have any comments about Theme 3?

*Theme 4: Landscape and Place Activation*

Do you have any comments about Theme 4?

*Theme 5: Community and Leisure*

Do you have any comments on Theme 5?

## Precincts

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

*[Precinct 1: Chute Street Commercial Strip](#)*

Do you have any comments on Precinct 1?

*[Precinct 2: Creek-side Open Spaces](#)*

Do you have any comments on Precinct 2?

*[Precinct 3: Elizabeth Street / Station Street](#)*

Do you have any comments about Precinct 3?

*[Precinct 4: Retail Core](#)*

Do you have any comments about Precinct 4?

*[Precinct 5: East of George Street, North of Main Hurstbridge Road](#)*

Do you have any comments about Precinct 5?

*[Precinct 6: East of Wensley Street, South of Main Hurstbridge Road](#)*



Do you have any comments about Precinct 6?

*Precinct 7: Schools and Recreation*

Do you have any comments about Precinct 7?

Do you have any other comments about the draft Structure Plan?

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Wednesday, 29 April 2020 10:50:16 PM

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## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

**Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

The Vision sounds good but the draft plan fails to support the vision. The draft plan focuses way too much on increased new development over simple and practical measures for improving and enhancing existing amenities.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *[Theme 1: Land Use Activities and Development](#)*

**Do you have any comments about Theme 1?**

### *[Theme 2: Built Form Character](#)*

**Do you have any comments about Theme 2?**

### *[Theme 3: Movement and Access](#)*

**Do you have any comments about Theme 3?**

### Theme 4: Landscape and Place Activation

Do you have any comments about Theme 4?

### Theme 5: Community and Leisure

Do you have any comments on Theme 5?

## **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full details, see the [draft plan](#).*

### Precinct 1: Chute Street Commercial Strip

Do you have any comments on Precinct 1?

### Precinct 2: Creek-side Open Spaces

Do you have any comments on Precinct 2?

### Precinct 3: Elizabeth Street / Station Street

Do you have any comments about Precinct 3?

### Precinct 4: Retail Core

Do you have any comments about Precinct 4?

### Precinct 5: East of George Street, North of Main Hurstbridge Road

Do you have any comments about Precinct 5?

### Precinct 6: East of Wensley Street, South of Main Hurstbridge Road

Do you have any comments about Precinct 6?

## Precinct 7: Schools and Recreation

**Do you have any comments about Precinct 7?**

**Do you have any other comments about the draft Structure Plan?**

I am strongly opposed to any maximum building heights over 2-3 stories in all precincts. Buildings over 2-3 stories are incompatible with the existing rural village character of Diamond Creek. The examples of the proposed skyrail area are horrendously ugly. I oppose any rezoning or amendment in precinct 3. I oppose duplication of Gibson St bridge. There is no need for a bike/pedestrian underpass under Main St connecting the trail but I wouldn't be opposed to a low impact cantilevered walkway under the bridge, provided its heritage status allows. Solutions for improving traffic flow and car park layouts, along Main st and in precinct 4, are significantly unclear. I support the ideas to improve bike paths, pedestrian paths, introduce cycle lanes and making Station St one way.

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**From:** [no-reply@harvestdp.com](mailto:no-reply@harvestdp.com)  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan - feedback form Form Submission  
**Date:** Wednesday, 29 April 2020 10:50:26 PM

---

## Diamond Creek Structure Plan - feedback form Form Submission

There has been a submission of the form Diamond Creek Structure Plan - feedback form through your Participate Nillumbik website.

### **Do you have any comments about the vision for the Diamond Creek Major Activity Centre?**

Yes I find the vision way out of step with what the actual plan presents. This vision is great on paper however how are you going to achieve the last sentence of your vision 'It will be known for its distinctive rural village charm, environmental qualities and safe and walkable street network. ', when you are looking to build a skyrail and 5 storey buildings. Visions are meant to tie in to the plan and unfortunately this does not. However some development would be great however it needs to be in step with the 'rural village charm'.

## Themes

*The Vision for the Activity Centre is achieved by the realisation of Objectives and Strategies grouped under five themes. Click on the blue links for a summary of each theme. For full details, see the [draft plan](#).*

### *Theme 1: Land Use Activities and Development*

#### **Do you have any comments about Theme 1?**

The general employment objectives seem reasonable and I believe would be great for the local economy. However the higher density living, I believe exact number would be needed because again this is a town with a rural feel. Increased residential development could very easily destroy this.

## Theme 2: Built Form Character

### **Do you have any comments about Theme 2?**

Absolutely horrendous the plan to have for one, 5 storey buildings, but that amount is horrible. I don't know how you can keep the semi rural character when your plan wants to make as a Preston overnight. I think 3 storey building would be a lot better and maintain the character. They would not be as imposing and would not look out of place.

## Theme 3: Movement and Access

### **Do you have any comments about Theme 3?**

Again out of step with reality. Any development plan should only come after increasing or planning of the increase in lanes from 1 to 2 along the shopping precinct. Skyrail would be an eye sore and would totally erode why people live in diamond creek. The reservoir sky rail, put that in a rural town like diamond creek and it would destroy the fabric of the place. Further in the February Council meeting it was mentioned that the rail cannot go underground due to flooding. 1 surely in this day and age that issue can be solved, it may cost more but you would leave product that locals would get on board. and 2 it is then mentioned you want to put an underpass connecting the DK trail, how often will this be flooded? Also If you are planning to increase the use of the precinct car parking should be a priority. People drive no matter how far or close they are the majority of people drive so to reduce the dependancy on parking is not what actually happens.

## Theme 4: Landscape and Place Activation

### **Do you have any comments about Theme 4?**

No issues any increased gardens. Would also be good to increase playgrounds in the area with all the development of new house and young couples and families moving into the area.

## Theme 5: Community and Leisure

### **Do you have any comments on Theme 5?**

Those objectives and strategies would help the area.

# **Precincts**

*The activity centre comprises 7 precincts, each with a preferred character, objectives, strategies and design guidelines.*

*You can see the [full Precinct Plan here](#).*

*Click on the blue links below for a summary of each precinct. For full*



details, see the draft plan.

### Precinct 1: Chute Street Commercial Strip

#### **Do you have any comments on Precinct 1?**

Again size of the development an issue and no plan for parking and would have to much of a concrete jungle feel.

### Precinct 2: Creek-side Open Spaces

#### **Do you have any comments on Precinct 2?**

Bike path to hurstbrige would be great The underpass not sure how well that would go if the area is flood prone Possible library would be great, the eltham library is still quite popular. Reducing the speed limit but not increasing the lanes would cause chaos.

### Precinct 3: Elizabeth Street / Station Street

#### **Do you have any comments about Precinct 3?**

Duplication of the bridge is must, especially due to increase traffic flow from all the new development in collard. 5 Storeys way out of step with the area and the sheer scale.

### Precinct 4: Retail Core

#### **Do you have any comments about Precinct 4?**

Extra parking would be required, along the main shop. Lowering to 40km from Brooks Cres to Bruford Street would make the precinct safer however you would need to make that part of the road 2 lanes both ways

### Precinct 5: East of George Street, North of Main Hurstbridge Road

#### **Do you have any comments about Precinct 5?**

Again heights and scale does not fit the character of the area

### Precinct 6: East of Wensley Street, South of Main Hurstbridge Road

#### **Do you have any comments about Precinct 6?**

Way to much residential development and the height of those developments would be out of place in Diamond Creek

### Precinct 7: Schools and Recreation

#### **Do you have any comments about Precinct 7?**

All seems reasonable, better links will make it safer.

**Do you have any other comments about the draft Structure Plan?**

I can understand that this is just a plan, and I for one would be happy with development of the precinct, increased office space (I would love one day to open and run my own business in DK), and small scale residential development. However the sheer scale is something that myself and many locals are not happy with. Sky rails, 5 storey buildings and keeping the road 1 lane each way along the shopping precinct would achieve an undesirable outcome. The scale and size of the developments should better reflect the area Also all locals should actually be informed of any structure plan. It it means sending out lots of letter will so be it, we deserve to have out say.

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**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Objections  
**Date:** Friday, 24 April 2020 11:38:25 AM

---

Hi Julie Padget

Why is Nillumbil Council allowing a 4 storey businesses and apartments in Main Hurstbridge Road to be built.

I live in [REDACTED] Diamond Creek for 57 years and the extra traffic in Gregg Street would double the through traffic.

I strongly object to this building going ahead.

We supposedly live in the most liveable area in Australia not so if this goes ahead.

Regards [REDACTED]

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Possible re-zoning in diamond creek.  
**Date:** Sunday, 26 April 2020 11:43:42 AM

---

Dear Julie,,,,I am writing in response to a letter left in my letterbox,concerning possible rezoning in diamond creek.(re area 6). I am totally against the proposed developments for many reasons. Diamond creek main rd is already highly congested and very slow,especially during peak hour,,and school drop off and pick up times.So,,,any development that will add to that congestion seems crazy!! Also,lately,many of the previous residential properties along that stretch of main rd have been turned into commercial ventures,,,which seems totally viable,as no-one would chose to live in such a busy rd.

As for the proposal of 4 storey constructions along that same stretch of road!! I am shocked!!and disappointed with the council (whom I have paid rates to for the past 33yrs),,,that they would even consider to totally change the atmosphere of this area,that has attracted people to live here.Apart from even more traffic,more car fume pollution,more noise,a much less environmentally pleasing town,lack of privacy for adjoining residents,,and no doubt many more inconveniences for all of us who live nearby.

So,,really ,I am just voicing my opinion, that I am totally against any more of this type of development in diamond creek.We are ,the so -called,green,environmentally aware,semi rural,quaint, little unique town ,on the edge of the growing urban sprawl,surely you would be doing your absolute best to keep it that way!????? Not give in to the lure of some multi level development projects just to obtain more rates!!!

I feel that if this goes ahead ,I,and many others will be forced to leave our loved homes ,of many,many years,,to search out a place to live where they do consider the whole atmosphere of a town,,,not just what they can squeeze into,and out of ,it!!

Yours sincerely.... [REDACTED]



**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Diamond creek activity centre review  
**Date:** Sunday, 26 April 2020 8:26:51 PM

---

Hello

To whom it may concern.

I have read the activity centre proposal and as much I think that diamond creek needs to adapt and change for the future, this very big grand plan may take away from the small country town feel of diamond creek.

There are certainly some good things in the proposal.

- the need for a footpath and parking on the oval side of Elizabeth st is a must. I work [REDACTED] on Elizabeth st and if the parking was put in on the park side of Elizabeth st this would certainly help with making the Rd wider and allowing for the cars and the public busses to all get down the street without much fuss. If the parking was put on the park side (there is plenty of room) and the factory side was made no standing the traffic would flow and would also make it safer to get in and out of the driveways in this street. A child was killed some years ago in this street due to a car not seeing a child step out from behind a parked car. Due to the flow of traffic and visibility issues that already exist I fear this may happen again as most weekday afternoons the street is very busy with kids and parents attending gyms and the pool in summer.

- the proposal suggests looking into making station street one way heading from the station back towards the pub. As someone who is around and using the roads around station and Elizabeth street this would be kaos. If anything it should be one way heading from the pub towards the station. This way people can go around the block and head back out onto the main road in any direction left or right. Currently there is no right turn out of station street into the main Rd which is great. But if it was one way heading towards the pub and no right turn then people would be forced left if they wish to go right and would cause frustration and many other traffic issues of people going up back streets and doing u turns on the other side of train tracks so they can go where they wish to go.

- More bike paths and Bridges over train line in different areas is a great idea.

- most importantly the need to remove the train line level crossing is a big must. All emergency services police, fire, ambos are on the coles side of the train line. In the event of an emergency being it fire or medical, if the first responders cannot get to us on the other side of train line due to the train coming and the boom gates coming down this could be the difference between life and death. The other day a fire truck nearly Went around the boom gates that were down and the train horn warned them and they stopped. They nearly got hit by the train.

If everyone has to put up with some small time pain to get the long term gain of having the level crossing removed then so be it. It will be worth it.

As for the allowance of up to

Five stories in the current industrial and shopping centres. This would block out sunlight, take away the small town feel and country feel of diamond creek.

Any questions or input you would like from a person who operates out of the Elizabeth street precinct please don't hesitate to contact me.

[REDACTED]

Sent from my iPhone



**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** DIAMOND CREEK - ZONING DEVELOPMENT AREA 6 [SEC=UNCLASSIFIED]  
**Date:** Monday, 27 April 2020 9:59:59 AM

---

UNCLASSIFIED

Attention

Julie Padget

I am lodging an objection to the study map Area 6 which has allowed the construction of multi storey buildings.

I have been a resident of Diamond Creek for over 30 years and this type of development is not suitable for the area.

Some of the reasons are:

1. Four storey buildings will overlook our properties, homes and back yards in Gregg Street . We Will have no privacy.
2. This could double the through traffic in Gregg Street as wells as other streets. This a real concern for the safety of my children and other children living in the street.
3. Main Hurstbridge Road is a single lane and there is no turn lane room in any properties presently
4. There is no parking in Main Road , it will create the need for a lot of extra parking and we may find the school parking could be effected

My family and I would like the Council reject the building of 4 storey buildings in the Area 6 of the study map

[REDACTED]

Diamond Creek Resident

Contact Mobile :

[REDACTED]  
[REDACTED]  
[REDACTED]  
[REDACTED]

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Major Activity Centre - attention Julie Padget  
**Date:** Monday, 27 April 2020 12:11:04 PM

---

Dear Julie

It was brought to my attention via a local letter drop of the intention to create the Diamond Creek Major Activity Centre.

I live in [REDACTED] and my house backs onto [REDACTED]. I have lived here for six years and chose the location due to its close proximity to local shops and amenities as well as public transport.

Looking at the plans for this centre there are a number of issues that cause me concern. Firstly, the intrusion of multi level properties close to my home that will cause me a loss of privacy not to mention extra noise. As it is, there is substantial noise that comes from the [REDACTED], but as they were there before me, I cannot complain about that. Working from home too at the moment, I can hear the noises coming from the child care centre. I have minimal privacy in my backyard as it is because of the number of people visiting the [REDACTED], but again, they were there before me.

If the area along Main Road is developed as per your plans, this will cause significant problems for my wellbeing with increased noise, lack of privacy and possibly significant shading due to buildings blocking out my sunlight.

Despite them saying they will have underground carparks, there are never enough carparks during these constructions. It mentions on street parking, but there is no on street parking in the area at the rear of my property. They also mention there will be increased trains - as a regular train traveller I know there are no plans to upgrade the crossing, no plans to duplicate the line and no plans to increase trains along our line. If they did intend to simply add more train services, this would impact severely on traffic as that crossing is the main arterial and so much traffic comes through as it is. On weekends and during peak times, it can often take up to 20 minutes just to get to Yan Yean Road.

I was unaware of these plans until the letter was handed to me and I then went onto the council website to look at the intentions. I think it's poor that us residents who live so close by have not been given any information in the time I have been here. Traffic comes through Gregg Street during peak times as it is, with people trying to beat the traffic on the main. They are speeding as it is, despite the speed humps. If more office and residences are placed where I believe it's going, all this will do is further endanger us who live in this street. There are often children playing on the road, and they should be able to do this safely, if it's only residents who use the road.

I cannot in all honesty see exactly what is planned, but I believe we should have some input if the development is to go ahead as I've been led to believe with multi-storey

business and apartments.

Sincerely

[REDACTED]  
[REDACTED]

Diamond Creek

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Major Activity Centre  
**Date:** Monday, 27 April 2020 7:53:34 PM

---

Dear Julie Padget

I'm writing to object the proposal of the building for 4 storey business and apartments in Main Hurstbridge Rd Diamond Creek.

With having grown up and being a Diamond Creek resident for more than 45 years, I certainly am concerned of this happening, I live at unit [REDACTED] Diamond Creek and at the moment our street is already full with resident car parking in the street and also at the end is Bruford St and this is already cluttered with people that catch the train.

I'm already horrified of how our Diamond Creek community has changed over the last 15 years, I understand progress but this plan of building such a large building will put some big strain on all concerned. This will dramatically have a bit impact on traffic, train crossing and the residents within the vicinity, not to mention the children walking home from the nearby schools..

Please have this stopped, as I'm a 5th generation Diamond Creek person and grew up in Ironbark rd diamond creek.. My mother, great grandparents and lots of relatives are buried in Nillumbik cemetery and I know they would turn over in the graves if this was to happen. My Great Grandparents home still exists at the top of Collins ST.. It's disappointing that us Diamond Creekers have lost some of its ambience to progress, but please we need to hang on with what we have left. I say no to this building .

Kind regards

[REDACTED]  
[REDACTED]



**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Structure Plan Precinct 6 - feedback  
**Date:** Tuesday, 28 April 2020 12:53:45 PM

---

Hi Julie,

In addition to our previous feedback submission.

Regarding Council's objectives and strategies the plan submitted by Nillumbik Shire and the Victorian Government seem to be in contradiction. Three and four story developments in Main Hurstbridge Road (Precinct 6) will not integrate with the existing character of the area.

We built our home in [REDACTED] in 1973 in a residential area which allowed for families and children the use of their backyards rather than the alternative of playing in the streets.

We both worked in the city but we purposely built our home in an area which was not congested like all the inner suburbs.

The site to build three and four story businesses and apartments is wrong for more than one reason.

1. The Main Rd is too narrow and already heavily congested at most times.
2. This will cause additional traffic through the side streets and mainly Gregg. Additional through traffic will cause safety concerns mainly for the children playing in the street and commuting to and from school.
3. The appearance of these apartments are not suitable to this residential area.
4. Privacy is also a major concern. If this construction is allowed to go ahead they would require construction of screens, any windows would require frosting, any balconies would have to be screened off also.

Several people have suggested an alternative site on the vacant lands in Main Hurstbridge Rd opposite the area of Illanaview Drv near the ambulance station. This would not encroach on anybody's privacy. Multistory buildings in this area would blend in more suitably. This seems to be more in line with your objectives and strategies whereas precinct 6 should really be predominantly residential.

Before any of these works are even considered the traffic congestion has to be addressed and the rail system would go a long way to achieving this. It is now approximately 8 years ago that I spoke to a member of the Victorian Labour party with concerns in the Diamond Creek area. The main points we discussed was that Vicrail rectify the single track system through to Hurstbridge which would allow a lot more people to use the train system rather than the congestion on the roads. His response at the time was that nobody had informed his government of any problem with the Hurstbridge line.

To date nothing has been done.

[Regards,](#)

[REDACTED]

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Activity Centre - Draft Structure Plan Feedback  
**Date:** Tuesday, 28 April 2020 4:46:20 PM

---

To whom it may concern,

I have lived in Diamond Creek since birth (28 years), and love the rural charm and small town community feel. When I moved out of my parents' house, my husband and I decided to buy a house in Diamond Creek in 2015. We live here because we love the small town feel. I live in [REDACTED] (behind Main Hurstbridge Road), and the proposed developments would directly impact my daily life. I have a newborn daughter, and plan to live in Diamond Creek for the rest of my life while raising my family.

Diamond Creek has changed over the years, especially with how busy it has become, but it has kept the rural charm to date. Higher density development, both commercially and residential, will remove this rural feel. Diamond Creek roads are already congested, particularly in peak hour and on a Saturday morning. Adding more residential dwellings, and a plaza, would bring this town to a halt. Building 4 and 5 storey buildings will completely change the atmosphere of Diamond Creek- making it an inner city suburb. My personal view of the trees and skyline will be directly impacted by the 4 storey buildings on Main Hurstbridge Road opposite Mitre 10.

My main concerns are as follows:

1. The proposed plaza and 4-5 storey buildings will remove the rural charm of Diamond Creek.
2. By adding 303 residential dwellings, on top of the commercial buildings, will congest our roads. Diamond Creek roads are already too busy, and this development cannot go ahead without making provisions to add lanes.
3. Diamond Creek is in a bushfire risk zone (as stated on page 27), and no extra roads or lanes would endanger the lives of those that live in Diamond Creek.

I approve of the following developments:

1. Adding a bicycle lane along Main Hurstbridge Road. Currently it is dangerous for the cyclists and drivers, and it slows traffic in the area.
2. The Sky Rail for the train line does need to be built, however it needs to reflect the rural charm of Diamond Creek (instead of looking like Rosanna and Mernda stations).

Regards,

[REDACTED]

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Diamond Creek Activity Centre - Draft Structure Plan Feedback  
**Date:** Tuesday, 28 April 2020 6:04:36 PM

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To whom it may concern,

I have lived in Diamond Creek for the last 5 years, and love the rural charm and small town community feel. When I moved out of my parents' house, my wife and I decided to buy a house in Diamond Creek in 2015. We live here because we love the small town feel. I live in [REDACTED] (behind Main Hurstbridge Road), and the proposed developments would directly impact my daily life. I have a newborn daughter, and plan to live in Diamond Creek for the rest of my life while raising my family.

Diamond Creek has changed over the years, especially with how busy it has become, but it has kept the rural charm to date. Higher density development, both commercially and residential, will remove this rural feel. Diamond Creek roads are already congested, particularly in peak hour and on a Saturday morning. Adding more residential dwellings, and a plaza, would bring this town to a halt. Building 4 and 5 storey buildings will completely change the atmosphere of Diamond Creek- making it an inner city suburb. My personal view of the trees and skyline will be directly impacted by the 4 storey buildings on Main Hurstbridge Road opposite Mitre 10.

My main concerns are as follows:

1. The proposed plaza and 4-5 storey buildings will remove the rural charm of Diamond Creek.
2. By adding 303 residential dwellings, on top of the commercial buildings, will congest our roads. Diamond Creek roads are already too busy, and this development cannot go ahead without making provisions to add lanes.
3. Diamond Creek is in a bushfire risk zone (as stated on page 27), and no extra roads or lanes would endanger the lives of those that live in Diamond Creek.

I approve of the following developments:

1. Adding a bicycle lane along Main Hurstbridge Road. Currently it is dangerous for the cyclists and drivers, and it slows traffic in the area.
2. The Sky Rail for the train line does need to be built, however it needs to reflect the rural charm of Diamond Creek (instead of looking like Rosanna and Mernda stations).

Regards,





Julie Padget

Strategic Planning

Nillumbik Council

**Re Nillumbik Structure Plan**

Dear Julie

I am writing to object to the Diamond Creek Major Activity Centre Area Number 6 legislation that allows for the construction of multi storey buildings, such as 4 storey businesses and apartments.

1. Multi storied buildings will undermine the amenity of the area current residents enjoy.
2. Overshadowing will effect residents of Gregg Street whose properties back onto the properties in question that are on the south east side of Main Road.
3. The privacy of these residents will be impacted.
4. There will also be problems associated with the resultant increase in traffic and associated parking needs.

I have been a resident of [REDACTED] for the past five years. Obviously the Council saw fit to prevent the street I now reside in being a rat run for cars as it put in place speed humps within the street. I fear that, with the construction of multi storey buildings on Main Road, will come the need for more street parking in the area from the resultant increase of traffic and, as the street I live in and the surrounding streets have families with both children and elderly folk residing in them I am fearful for the safety of the residents. It is an area near a school and a community hall used by many residents.

It would seem to me that rescindment of the current legislation would be the better way to go in regard to this matter. Multi storeyed buildings would be better accommodated in the shopping precinct where overshadowing and privacy would not cause problems and parking could easily be accommodated.

Please take my concerns into account

Thanking you for the opportunity to comment

[REDACTED]

[REDACTED]

Diamond Creek 3089

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Julie Padget - 2020 Strategic Plan  
**Date:** Tuesday, 28 April 2020 11:07:05 PM  
**Importance:** High

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Tuesday 28th April, 2020

Dear Julie Paget and to Whom it May Concern,

I am writing to you as a lifelong resident of [REDACTED], Diamond Creek and on behalf of my parents who have lived in [REDACTED] for 37 years.

The idea that multi-storey buildings would be considered in this already crowded and busy area of Diamond Creek is beyond belief.

These plans were made in 2006, in the 14 years since, Gregg Street has gone from being a place where children could play safely in the street, to a place where cars have to park on the side of the street due to ill-conceived planning permissions, and the splitting of residential blocks, to make room for two dwellings on a single block. Shouldn't we be preserving the wonder that is Diamond Creek's community?

Children and teenagers in the street are able to walk safely to the local primary schools and secondary schools. The elderly are able to walk to and from the shops and doctors with ease. How is the safety of the community being factored into these plans, when the overflow of traffic from Main Road will be coming through Gregg Street? Why would we want to have more traffic in an already built up area?

The overflow of traffic from Main Road to Gregg Street is a genuine concern in this 2020 Strategic Plan. Myself and my family have seen the changes in Gregg Street, there are already two speed humps in the street supposedly to slow the traffic down. As you can imagine, this already creates parking issues. Social gatherings and celebrations create parking and safety issues in Gregg Street. Waiting for cars to enter Gregg Street from Bruford Street often creates difficult manoeuvres for passing traffic. Turning onto Main Road is already dangerous, so to have more traffic on Main Road and using Bruford, Gregg Street and Phipps Crescent is making the traffic in that area more hazardous. It's difficult and dangerous during the day, let alone peak hour. Have you tried to get through Diamond Creek to Greensborough on a Saturday morning? What about travelling home to Diamond Creek in peak hour during the week?

Instead, the plans that have been proposed are going to ensure that the unique 'community feeling' of Diamond Creek will be lost in amongst oversized and grandiose multi-storey buildings, which will impinge on the privacy of Gregg Street residents. The community feeling and knowing your neighbours will be lost. These are simple ideas which will be lost as people keep to themselves, as more people are crammed into multi-storey buildings. That's not Diamond Creek's neighbourly way of knowing those in your street.

How is this going to improve the quality of the lives of Gregg Street and surrounding residents? Or is this the plan? Get rid of Gregg Street residents and continue the horrific degradation of Diamond Creek? Is Main Road just the beginning? What are the plans for 2050? The entire community living in one another's pockets, more than I kely.

So much for 'The Green Wedge'.

Sincerely,

[REDACTED]  
**IMPORTANT** - This email and any attachments may be confidential. If received in error, please contact us and delete all copies. Before opening or using attachments check them for viruses and defects. Regardless of any loss, damage or consequence, whether caused by the negligence of the sender or not, resulting directly or indirectly from the use of any attached files our liability is limited to resupplying any affected attachments. Any representations or opinions expressed are those of the individual sender, and not necessarily those of the [REDACTED]

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Major activity centre  
**Date:** Wednesday, 29 April 2020 11:37:42 AM

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Dear Julie,

I have concerns regarding the changes to the Major activity centre in Diamond Creek.

The beauty of Diamond Creek is it's 'small country town' feel, these changes would ruin the entire feel and appearance of the suburb. Extending the current (already disappointing) regulation allowing three story to now four storey buildings would be a huge shame for our community. Main Hurstbridge rd is a single lane, already teeming with traffic and regularly at a stand still when a train goes past. The frequency of trains will increase, as will the traffic - adding towering buildings is only going to increase this traffic.

These buildings would affect me personally as they would overlook my own house and cut out a large amount of natural light coming into my home. People have chosen to live in Diamond Creek for it's space and country like appearance, if we wanted to live in the city with multi story buildings we would have. Please don't allow the integrity of Diamond creek to be ruined!

[REDACTED]  
Resident of [REDACTED] Diamond Creek

Dear Julie Padget,

Regarding Nillumbik Structure Plan for Diamond Creek Major Activity Centre.

I would like to draw your attention to a number of factors as to why any development associated with 'Area Number 6' would be a disastrous idea.

Some background on this particular resident:

- I'm part of a fourth generation, on one side of my family, and third generation on the other side who have lived in the Nillumbik area.
- I have lived in [REDACTED] for 33 years and specifically, my current residence, since January 29<sup>th</sup>, 1999 and was built by my grandparents in 1960 and my maternal great grandparents also built a house in Gregg Street, which was sadly knocked down for units in 2012. My grandparents have also lived in Gregg Street since 1960 but have resided in a different property, [REDACTED] since October 1999. I have extreme sentimental value for my home and will hate to see it destroyed by over the top developments.
- In this time, I've seen a lot of change in the Diamond Creek area and there have been a number of changes that I don't believe have been for the suburbs best interest.

In regards to the Nillumbik Structure Plan for Diamond Creek Major Activity Centre, which objections close on 29<sup>th</sup> April, 2020, there are a number of concerns which I believe the Council need to be fully aware of.

## Please be aware that all of the following points regarding traffic congestion are based prior to the outbreak of COVID-19, when there were considerably higher volumes of traffic on the road inclusive of local, state and national scales. Current levels of congestion, as in all of April 2020, should truly be excluded from discussions.

In relation to  
Gregg St:

- Due to the local council already permitting the subdivisions of properties, there has been an increase of traffic in Gregg Street for 8 plus years. This is inclusive of residents trying to gain access to their properties (owning or renting) or people using the street for a shortcut to avoid traffic.
- Additionally, neighbouring streets are also feeling the impact of increasing traffic.
- Similarly, the increase of traffic whether it is through or not, raises safety concerns. These concerns are for the children who live in the street and also for the number of over 65 years that live in the street who are not as quick as they once were.
- There is also an increase of vehicles parking in Bruford Street (specifically workers from the Story House Early Learning) which makes access to Gregg Street dangerous due to an increase of blind spots at the T-way intersection of the two streets. This will only become worse. I specifically have had multiple close calls with idiotic drivers who are oblivious to residents, other vehicles (parked or not) and pedestrians as they speed through the streets.
- There has been an increase of crime in the area, irrespective of the fact that we are within a 2 minute walk to the police station. (On a side note, when the police station was built, it was promoted as going to be operational 24 hour a day. It is not. The police are always on patrol and never in the station when you need to speak to them. Also, I recently found the paperwork relating to the Diamond Creek Police Station which does state 24 hours operational.)



In relation to 'Area Number 6', in a broader sense:

- Any multilevel structure which is built will only allow privacy for those residents living in the multilevel structures and will subsequently ignore any rights to privacy that existing residents previously had prior to any new developments made in the area. Everyone single resident in the Nillumbik area deserves the right to privacy in their existing properties. New structures would see this basic societal and human right disappear from existing residents.
- As an extension of the above point – Litter and or rubbish. Multilevel structures increase litter and or rubbish. I am specifically concerned as to how control of litter/rubbish will be monitored both in relation to Main Road the extremely obvious fact that litter/rubbish will find its way into existing properties' form Multilevel structures. Truthfully, if I know for a fact that rubbish on my property is not mine, I will send it back to where I believe that it belongs and if absolutely necessary I will lodge further complaints to the Council.
- Multilevel structure development will increase the crime rate for any existing properties, irrespective of any security measures which could be put in place. Existing residents and properties will suffer the most through all aspects of crime.
- All multilevel structures should be put into the newer housing estates or even ones that are still waiting for construction to occur as a more logical spot and better space to successful developments.
- Any development which 'could occur' along Main Road, would cause more congestion on the roads, which is already to put it very mildly, quite bad. Peak hours, school times and weekends, you name the time and it's rare to see Main Road quiet. This is further evident by public transport, specifically buses that have stops on Main Road.
- Traffic and parking are already in limited supply along Main Road in areas where it is somewhat safe to park. E.g. Near Diamond Valley College. Furthermore, as there have been absolutely no attempts by the Council decades prior to the beginning of the 21<sup>st</sup> century to widen Main Road, there is now no real logical reason to do so now as it would cause a traffic congestion nightmare in any attempt to fix the roads. Not to mention the ridiculous concept of a 'landscaped civic spine' will also aid to the traffic and road congestion as the road as it currently stands is neither safe nor wide enough for anything to be done, the time has long passed to widen the road. This point also has direct ties to impacting Gregg Street as well as neighbouring streets as drivers will abscond from being stuck in roadworks on Main Road and subsequently congest the surrounding streets.
- Additionally, while I praise people who are aiming to be environmentally conscious, but developments to include bike lanes would further narrow Main Road and add to the substantial traffic congestion.
- There is absolutely no need to reduce the speed limit along Main Road for 60km to 40km. It is just a vain attempt of the powers at be trying to enforce Eltham standards upon Diamond Creek. Diamond Creek is not Eltham and any and all redevelopment of the magnificent area of Diamond Creek will destroy the uniqueness that Diamond Creek already has in place.

In a general sense for redevelopment:

For my entire life, my parents and grandparents have shared this little nugget of gold about the Hurstbridge Line: 'since the 70's the Hurstbridge train line is going to be duplicated'. Simply, peak train travel will not be a train at Diamond Creek Station every 40 mins, whether it be Hurstbridge bound from the City or City bound. However, it would be more in line with the Epping (now Mernda) Service of at maximum 20mins in between trains. These members of my family are still waiting to see the total duplication of the Hurstbridge line to occur.

Ultimately, any modernization of Diamond Creek will lose the charm that it already has. This is a unique place as it is and the idea of jazzing it up to reflect other suburbs and or townships will ruin the already strong community values and ideals that exists in the area.

I love driving and walking through Main Road and seeing the gorgeous gum trees that line the area. I don't want to see this fabulous feature lost through redevelopment, which sadly it will be should the proposed plans be allowed.

Don't ruin the area.

Kind regards.

██████████

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Diamond creek revamp  
**Date:** Wednesday, 29 April 2020 1:02:49 PM

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## Diamond creek

Well if you want to read my 2 cents worth. I agree with an elevated train track like Mernda. Parking underneath and access to either side of the train line from the industrial side to the retail side. Elevated over the level crossing which should have been 4 lanes before they built Coles, 30 years ago. The road was designed for 4 lanes out to Wattle Glen roundabout but never built. I am Not in agreement with any housing built on the railway parking area. There are enough problems parking near all railway stations. It may be necessary to build the elevated station behind M10 and the Bus depot with ramps back to Coles and on to the sports centre.

Not happy with play areas being mixed up with commuters it needs to have parking. Even a compound for bikes

[REDACTED]

Sent from [REDACTED] Not very good at spelling smart phone

**From:** [REDACTED]  
**To:** [Strategic Planning](#)  
**Subject:** Nillumbik Structure Plan for the Diamond Creek Major Activity Centre  
**Date:** Wednesday, 29 April 2020 3:58:05 PM

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Dear Julie,

Regarding Nillumbik Structure Plan for the Diamond Creek Major Activity Centre.

Re: Area 6 of the study map to allow 4 storey businesses and dwellings:

I object on the following grounds:

1. Main Hurstbridge road is a single lane and very narrow. Traffic is frequently at a stand still. Some drivers try to avoid this road block by using the side streets (particularly Gregg street [REDACTED]). There is no parking on the main road so again drivers use the side streets as do local business workers.
2. Vic rail are going to increase peak hour trains to Diamond Creek but there are no current plans to update the railway crossing or parking
3. I prefer to use public transport to go to Watermarc in Greensborough on the bus. I cannot go to any exercise class starting before 10am due to the earlier buses regularly being delayed by the peak hour traffic and railway gates.

Allowing this large increase in density will have significant negative effects on the Ambience of the area and my lifestyle.

Regards,

[REDACTED]



Response to Nillumbik's Diamond Creek Draft Planning Document

[REDACTED], Diamond Creek

I would like to see a library incorporated into the Diamond Creek Leisure buildings along with a community creative art studio (or, alternatively, nearby). I would like to see the unique semi-rural/eco- sustainable bent (combined with our local Orchard and mining history) of this region maintained and expanded upon in style and development.

I am concerned about the development of high density housing and the 4-5 story draft recommendations. I believe that these will negatively impact upon the character of the area. I am happy to travel to Greensborough/ Eltham and do not require Diamond Creek to provide all of my retail needs. I would prefer to see less franchised retail and more artisan and small family run businesses – to this end, I would like to see the Council work with the local retail/traders association to ensure that, especially with any upcoming new developments, that these small business owners don't continue to be priced out of renting local retail spaces.

#### **Theme#1: Land use**

I don't necessarily trust the draft's stated intention is to actually provide accessible and affordable housing options close to public transport. I believe that should these be built they would then still be expensive. I would like to see a clear explanation of what guarantees are being provided and what is the definition of "affordable" before these new residential constructions are approved and built.

I like the strategy of supporting Diamond Creek's artisanal food production but what assurances would be put into place to ensure that the developments won't just turn into MORE franchises? - we already have so many and they do impact negatively on the overall style of the local retail and community spaces.

*Encourage uses that extend the hours of activity of the activity centre - good.*

However, do we need to develop an evening economy in Diamond Creek or is this just a money making venture to encourage more CBD and mid-suburban residents to move out to Diamond Creek? This strategy doesn't feel like it is responsive to local needs & wants but, instead, is a strategy to increase income for council, developers and other stakeholders.

*Provide the opportunity for a transit-oriented development (TOD) on land to the west of the railway line (Precinct 4) including uses such as commercial and residential - I don't even understand what KIND of transit oriented development you are suggesting here.*

*Undertake community consultation to review the potential application of C3Z (or a modified AC22) in the existing light industrial Precincts 3 and 5 to allow for a diversification of uses from creative industries, small manufactures, start-up businesses to offices, to facilitate business growth and employment opportunities. - yes, please ensure that A LOT of community consultation occurs with a lot clearer promotion of this. This does not just mean a survey or involvement from local service clubs and the retail traders association but an opportunity for local (not in Greensborough but in Diamond Creek) people to meet and discuss what is being proposed BEFORE it is approved. This should include regulars Diamond Creek locals that live in outlying towns/hamlets such as Hurstbridge, Cottlesloe, Christmas Hills etc as they will all be affected by any changes and decisions.*

I am concerned about too much vertical growth in terms of building development - over 3 stories is concerning to me. I am VERY opposed to redevelopment and increased residential densities to provide additional dwellings within walking distance of the town centre. I feel that there is already too much change happening in streets within walking distance to town and would prefer that this slowed down - too many town houses have appeared in a very short period of time and this is changing the style of the suburb/town already and rapidly.

I like the idea of a local employment anchor in the Activity Centre- civic or health based or both as long as enough additional off street parking is also provided.

## **Theme #2:**

### **Built Form Character**

I think that building heights across the activity centre should be no greater than 4 storeys, preferably 3.

I think that the tallest buildings clustered around the train station and within the Elizabeth Precinct should be maximum 3-4 story. I don't believe that Diamond Creek should have any 5 story buildings or higher.

The character of the semi-rural/ artisan producers / artistic community / local environmental sustainable interests/ combined with our Orchard farms and gold mining history provide us with a unique opportunity to choose now to develop this further via ensuring that the character of our built forms remain true to this combination of values and styles. This means that we consciously decide to position ourselves as having a natural look and move away from overtly minimalist modern architecture and frontages.

Redevelopment along Main Hurstbridge Road should be built to the front boundary with car parking either provided in basements or behind the building and shouldn't be higher than 3 story. If there is further development of high development housing in Diamond Creek, the infrastructure will struggle to support it. We already have many many more people moving into the new developments over at Herberts Rise and nearer to Collins St/Broadgully Rd. Already this has increased cars on the road, people in and out of the Coles and IGA carparks and traffic at the Collins St/Main Rd lights, both Bridges as well as the railway crossing near Wattle Glen. Traffic turning right there will need a set of traffic lights or a roundabout in the very near future (Wilsons Rd).

I agree that upper levels should be recessive.

Heritage fabric should be retained. Upper levels must be setback behind the street wall in line with the heritage fabric retention.

I agree that redevelopment should respond to the architectural style and materials that complement the natural surroundings, vegetation and topography and, as a priority, should respond to the rural village character. In addition, local wildlife (including birdlife) should be considered and the gumtrees on the main Hurstbridge Rd should NOT be removed.

*Buildings should be designed to respond to the slope - yes, I agree.*

*Buildings front, side and rear setbacks should be responsive to the adjacent character and landscaping - yes, I agree.*

*Future redevelopment of the Diamond Creek Tavern should respect its heritage features. Built form adjacent should ensure it does not dominate the heritage form - yes, I agree.*

*Active frontages should be provided along Station Street, Elizabeth Street, Chute Street and Main Hurstbridge Road - yes, if needed.*

### **Theme#3:**

#### **Movement and Access framework plan**

*Investigate improvements to the intersection of Main Hurstbridge Road and Fyffe Street to reduce congestion and alleviate safety concerns - yes please. Use EXTENSIVE LOCAL community consultation especially of those directly affected (Hayley, Gregg, , Patyah, Brooks, James Cook and other nearby streets). Allow for these people to give CURRENT feedback as to positives and negatives of all proposals before actioning.*

*Liaise with the Department of Transport (DoT) to advocate for the removal of the level crossing and potential implementation of a sky rail as per the concept plan provided above and at Figure 8 in the plan - the Rosanna development is horrible and ugly. I would hate to see something similar. The diagrams look like what was built in 2018/2019 in Frankston and it would negatively impact on the semi-rural vintage style of the town/suburb. I believe that alternatives should be strongly considered. I understand that this is a difficult site to make many viable options for but I don't think we should just go with the easiest, most obvious and ugliest. The character of this area is important and can't be recaptured once lost.*

*Investigate the opportunity to make Station Street one way, to lessen conflicts between cars parking and pedestrians and reducing vehicular turning movements, minimising congestion - I agree but think that it would work better as a one-way street going in the other direction to that suggested.*

*Investigate implementation of indented parking on the western side of Elizabeth Street - good idea, that street is crowded and unsafe when taking children to Titans or local swimming pool/park.*

*Redesign or re-optimize existing commuter car park to improve safety as well as internal car park circulation - yes please. Build UP here if you must go upwards in levels of buildings and provide a lot of FREE parking please.*

*Liaise with DoT to advocate for the implementation of on-road cycle lanes on Main Hurstbridge Road - yes. I think it's great that we get so many local and visiting cyclists here especially on the weekend but it is unsafe for them particularly around the bends near the DC HS towards Wattle Glen. Better on-road cycle lanes would improve traffic flow and encourage more visitors and greater retail patronage to the area.*

*Liaise with Vic Track and DoT to investigate additional pedestrian/ cyclist bridge over rail to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond - Please ensure that you consider the many elderly residents and those with disabilities would find a pedestrian bridge difficult to navigate. Whilst pedestrian tunnels can be dark/unsafe, if a large- light-filled one were provided it would be more accessible for all.*

*Liaise with the DoT to improve existing bus stops along Main Hurstbridge Road.*

*Investigate the opportunity to provide a raised pedestrian crossing directly east of Fyffe Street, connecting the northern and southern sides of Main Hurstbridge Road. Couldnt this be a set of traffic lights?*

*Create a clear cycling route from the Train Station to Chute Street Trail - yes please and the Diamond Creek - this would increase safety and, again, improving this would also increase non-local visitors. Signage is also important.*

*Implement proposed bike path connecting the Diamond Creek Trail to Hurstbridge to the north east - yes, great idea. - yes. I think it's great that we get so many local and visiting cyclists here especially*

on the weekend but it is unsafe for them particularly around the bends near the DC HS towards Wattle Glen. Well maintained and signed alternate bike paths would encourage more visitors and greater retail patronage to the area. Additionally, could these be used as an opportunity to promote the region, facilities, local artists via targeted and tasteful signage?

*Implement lighting, paving, and landscaping through existing at grade car parks and across the railway line, ensuring they are safe and inviting spaces. Yes! Do this NOW, please.*

*Investigate the potential for an underpass connecting the Diamond Creek Trail under Main Hurstbridge Road, connecting the northern and southern open spaces - YES, please. Also include safe clear wide pathway in this with great lighting and local creative art to decorate as well as clear signage and maps at either end.*

Please fix up the toilet block near the playground/netball courts. It was built a few years ago and is ridiculously UNSAFE. Coming out from the toilets the pathway leads DIRECTLY to the street where a toddler can run out to the street and be hit by a car turning left around the almost blind corner straight toward a running child. I have seen almost 10 near misses at this site. One day a child will be killed here. This needs URGENT remedying. eg a simple fix would be a short fence at the curb so a child can't run straight down the path into turning traffic. I cannot believe that this wasn't considered at the planning, implementation, building stages or since and this makes me concerned about the intelligence and safety of design for this much larger local proposal.

*Provide bike and mobility scooter parking at highly frequented facilities such as supermarkets and the train station – yes and not on the footpaths – please provide clear signage that this is dangerous and blocks the disabled/elderly/parents with prams' access..*

*Reduce the speed limit along Main Hurstbridge Road to 40km/h - Yes. Consider a roundabout at Wensely street/Main Rd intersection.*

*- Provide a safe and non-congested environment in the Station Street precinct, particularly by considering traffic calming devices, pedestrian crossings, carefully-placed parking, etc. - yes please. I don't understand why we have such a big carpark at Coles but it is so unwieldy, disorganised and very difficult to get in and out from quickly (on foot or in a car).*

#### **Theme #4:**

##### **Landscape & Place Activation**

Strengthen the landscape character along Main Hurstbridge Road through the provision of mature trees and planting within the streetscape and public realm - yes but please do not remove existing mature trees.

Investigate the opportunity for a public space/ plaza and the 'heart' of the Activity Centre to be located adjacent to the train station north of Main Hurstbridge Road away from the traffic congestion. Yes and please ensure that Community Cultural Development artists such as myself are employed to ensure that creativity and local imagery are represented.

Prepare an Urban Design Framework (UDF) for Precinct 4 that incorporates a public space/ plaza and the 'heart' of the Activity Centre to the north of Main Hurstbridge Road away from traffic congestion. Please ensure that EXTENDED consultation occur regarding any outcomes from this.

Enhance the public realm with seating, shade trees and space for outdoor eating.

Implement footpaths along northern side of Station Street and Gipson Street, and between Station Street and the Coles Supermarket on Main Hurstbridge Road - YES!

Investigate the opportunity for a raised pedestrian crossing directly east of Fyffe Street, connecting the north and south sides of Main Hurstbridge Road - no. A set of traffic lights please. Raised pedestrian crossings are unsightly and are difficult for elderly and disabled (like myself) to traverse. Implement improvements to the existing railway pedestrian crossing.

Investigate the provision of additional pedestrian railway crossings to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond.

Investigate potential footpath upgrades and widening along Main Hurstbridge Road.

Investigate improved pedestrian infrastructure at the intersections of Cowin Street, Diamond Street and Main Hurstbridge Road - yes please.

Development should respond to the distinct landscape character in Precinct 5 through maintaining front setbacks and landscaping. The front setback should be a minimum of 5.5m measured from the front property boundary.

#### Community & Leisure, Theme #5:

- *Investigate the capacity of the existing Diamond Creek Community Centre to be refurbished to be a multi-purpose community facility that includes a variety of services and spaces for all age groups and potentially a library.* I believe that a community library in Diamond Creek would be a useful resource. However, I wonder whether that would impact on the usage of the Plenty Valley Library near Appollo Parkway which isn't terribly far to travel nor is Eltham (just 1 train stop and close to the station). If there were to be a library developed in Diamond Creek, it would be essential to provide a suitable amount of ADDITIONAL parking in that area and more clarity regarding available public transport. Also, walking there from the railway station, for example, is difficult and made longer by lights being further down (Elizabeth st) particularly for disabled, elderly or parents with prams. As we are in a creative (and green wedge) area I feel we should consider being a leader area thinking outside of the box and providing a resourced and staffed community art studio attached to a training café facility with herbs/vegetables/eggs provided by an attached or nearby (community garden) garden venture. A Not For Profit LLN provider (such as, for example, Jesuit Social Services/Jesuit Community College) could form a partnership (with perhaps the councils neighbourhood house) to provide accredited training within this facility.
- *An Accredited Access Consultant should review upgrades required to sports ovals, pavilions, netball, tennis and bowls facilities to ensure they are accessible to all.* In addition to an accredited access consultant and consultation with local residents and stakeholders, local residents (owners and renters, not just business people, service club representatives and council stakeholders) should be a part of a subcommittee for each of these upgrades as well as playground development.
- *Advocate for the implementation of the Diamond Creek Regional Play Space and ensure it encompasses facilities for lifetime play.*

I am assuming that the title REGIONAL Play-space indicates that this will be a destination large play area. If this is the case there are 6 main things I believe should be included:



1. Suitable parking, pathways and signage to service locals and additional (non-Diamond Creek) visitors
2. Range of activity areas for ALL ages, not just small children – ie BMX area, skate area, mud play area, sandpits with water play, creative play spaces (large notice boards etc)
3. Eco-friendly materials in line with our semi-rural location and large number of local and regional ratepayers being engaged in environmentally sustainable activities and interests- ie natural materials, reclaimed materials, actually growing aspects, local artists (not just one) engaged with both design AND decoration.
4. Local parent committee to oversee logical child-friendly and safety aspects (see following comments)
5. Suitable seating of a VARIETY OF HEIGHTS to comfortably and safely seat elderly, disabled and other local people with a range of different seating needs (not everyone needs the same type of seating so why not offer a variety) plus substantial undercover and shaded areas for sitting and playing
6. Undercover area available for outdoor-based nature play and creative workshops (not to be taken over or hired by personal and group training physical fitness groups)
7. Council needs to ensure that individuals driving “community” initiatives aren’t given the power to create exclusivity on common ground that we all pay rates for. Local service clubs assisting with setting up community spaces should not have the power of decision making over community recreation areas. Also, any services offered should not be just for those financially well enough to afford it. I speak here about the local tram renovation program where families were able to have their name on a brick for a fee – this excluded my family and many others (as we could not spare \$90+ before the cutoff deadline) and now I have an extremely disappointed child who will be even more upset when the area is completed and he sees that his name is not there despite growing up here. I feel that, while this is a nice activity, there could have been an option for local families who are struggling financially to be included or to commit to a brick and pay off the fee to council/elsewhere.

Again, I feel that local residents (owners and renters, not just business people, service club representatives and council stakeholders) – in particular, local parents, Community Cultural Development Artists and playwork experts should be a part of a subcommittee for each of these upgrades. Many of the times a playground has been redeveloped or built in the local area there has been an obvious lack of understanding of the needs of the modern family and also some basic safety issues have been overlooked: these have then had to be redone or, in some cases, have been left as unwieldy/uncomfortable and, in a few cases, unsafe.

*A few examples are:* **Playground in Eltham** (near the mini rail): shelter provided by Rotary (?) not the council is small and largely ineffective; sandpit was unsafe, with a high “sink” installed that needed modification; water pump fed water AWAY from sandpit and down a pathway rendering it useless; pathway through unestablished plants caused expensive planting to be trampled/killed quickly; lack of suitable seating (especially for parents with disabilities or grandparent carers) particularly close to the sandpit where children play for extended periods; no planting on either side of the “zip line” swings so multiple children

were knocked over running across the pathway from one activity to another; metal sculptures that are now broken, rusting; very little shaded play equipment. **North Eltham Adventure Playground:** Within a week of this reopening, my family saw six separate incidents where small children fell or were dangling and about to fall at rope bridge crossing (it has now been fixed). **Diamond Creek Netball playground refurbishment:** When using the public toilets a child exiting these can run along the pathway directly to the street in a matter of seconds. If they were to venture onto the road then oncoming traffic (left turning from Hurstbridge direction of main rd) would not see them nor be able to stop in time as it is a fairly blind corner. It is unfathomable that a short fence would not have been installed at the end of this path either originally or at least since the park was rebuilt. I have seen multiple near misses here and I don't even use that park very often as my child is 9 and it is more a small child's playground.

- *Advocate for improvements to the Diamond Creek Train to improve user safety and experience, including more drinking taps and improved lighting.* The train station could definitely be safer – particularly for the elderly, disabled and women. Clearer signage also regarding which side (which platform) to stand on as not all users are local residents and, if there is a push to increase visitors and posit the town as being an “artisan store” destination then clarity will be required especially around pathways and transport.
- *Investigate opportunities to create additional meeting spaces adjacent to Coventry Oval and Main Hurstbridge Road:* Great idea (but not 4 or 5 story) and to be accessible for public use.

## PRECINCTS

Precinct 1: I don't see a need for a raised pedestrian crossing on Diamond Street. Instead, traffic calming additions (the road is wide enough), clear signage, a short fence at the end of the path from the public toilets and greater visibility from city bound traffic turning left into Diamond street would be preferable. I like the idea of the public being able to walk along the creek frontage to Inglis street – would a pathway be provided here?

Precinct 2:

I believe that we need to ask for reduced speed limits on the Main Hurstbridge Road as well as providing more pedestrian crossings (not pedestrian bridges).

I would like to see the existing Diamond Creek Community Centre be refurbished to be a multi-purpose community facility that includes a variety of services and spaces for all age groups and a library, community art studio and gallery and training café facility (see notes above).

An underpass connecting the Diamond Creek Trail under Main Hurstbridge Road would be a great solution as would implementing the proposed bike path connecting the Diamond Creek Trail to Hurstbridge to the north east.

Pedestrian paths should be built/refurbished as a matter of urgency and should include clear suitable locally relevant signage.

I believe that an additional pedestrian crossing bridge should be provided to cross over the creek between just past the netball courts to just past the dog play area (near the new regional recreational area). This would create greater access and would feel safer for me as a local woman with a disability when returning from this area.

Precinct 3:

Buildings shouldn't go above 3 levels preferably.

Better FREE parking provision on Elizabeth Street. Do something about large vehicles blocking vision for exit from McDonald's carpark.

I believe that the traffic would flow better if Station Street were one way in the other direction. This would mean that there would not be cars exiting from here onto main rd but instead via the lights at Elizabeth Street.

Precinct 4:

A pedestrian/cyclist railway crossing at George Street is an excellent idea. The area at the end of George street and the pathway behind Coles is under-utilised space and extra crossings are urgently needed. I feel that MORE crossings should be considered (eg up near the mitre 10 site also)

- *Contain retail activity within a compact pedestrian friendly core bound by the railway, Brooks Crescent, Waigo Way and Wensley Street/ George Street – yes – but pedestrian friendly means an improvement on the current pedestrian movement across carparks on both sides. Maybe the carpark on the IGA side could have cars moving in one direction?*
- *Provide new retail and office space through more intensive, mixed use redevelopment – but NOT 5 story buildings. 3 story maximum is preferable.*
- *Encourage mixed use residential and office buildings north of the railway – ONLY if off street parking is included and the semi-rural and environmental “look” is a part of this. Overtly modern or very highly condensed residential is not a preference.*
- *Provide the opportunity for a transit-oriented development (TOD) on land to the west of the railway line (Vic Track land) including uses such as commercial and residential – I am unclear on what this means (ie what type of commercial uses would be approved etc)- but, again, don't like the prospect of any 5 level (or more) buildings in Diamond Creek.*
- *Encourage progressive upgrades of railway infrastructure and the station to meet anticipated growth in demand for public transport services (including implementation of a potential sky rail) – I do not like the images provided and believe that they would destroy*

the style and ambience of this town/suburb. Alternatives should be provided as options before a final decision is made.

- *Liaise with the DoT to advocate for the removal of the level crossing including confirmation of the preferred outcome – this outcome should be based on a range of options being provided for face to face transparent discussion by the local community.*
- *Investigate redeveloping at-grade car parks for mixed use development, with replacement and additional car parking (if required) integrated in basement car parking – yes. Pedestrian access and directional flow should be addressed as priorities.*
- *Investigate the opportunity for a raised pedestrian crossing directly east of Fyffe Street, connecting the north and south sides of Main Hurstbridge Road – no. I believe that this will be excluding of those with mobility issues and will look awful.*
- *Investigate improvements to station access and visibility, creating an inviting entrance. Investigate the provision of additional pedestrian railway crossings to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond. Investigate potential footpath upgrades and widening along Main Hurstbridge Road. Implement improvements to the existing railway pedestrian crossing.- yes to all these points, provided that there is clear signage and no heritage or established native plant loss.*
- *Prepare an Urban Design Framework (UDF) that incorporates a public space/ plaza and the ‘heart’ of the Activity Centre to the north of Main Hurstbridge Road away from traffic congestion – great idea but this should be with native plants and lots of sheltered seating or it could end up looking like a dodgy ugly Bundoora/frankston/footscray style plaza space.*

**Design Guidelines: Buildings fronting Main Hurstbridge Road should:**

- *Provide continuous weather protection (such as cantilevered canopies/verandahs) over footpaths – I agree with this, imperative to ensure the semi-rural aesthetic is maintained and improved upon (to return to former style of town/suburb)..*
- *Plant substantial native and exotic trees where appropriate to provide shade in car parks and screen car parking areas – this should be a requirement, not a suggestion.*
- *Front and side fencing is strongly discouraged. The use of roller shutters on shopfront windows and doors at street level is strongly discouraged - agreed*
- *If required, at-grade or deck car parking is encouraged behind developments with an emphasis on landscaping to minimise visual impact on adjoining residential areas – this is essential to reduce increased parking needs on the main rd.*
- *New development at 42 and 44 Main Hurstbridge Road should provide for the retention and improvement of the site as a local landmark building – what building is this? – it wasn’t marked on the precinct 4 map provided.*



To Nillumbik Shire Council

Tuesday 28<sup>th</sup> April, 2020

**Re: Nillumbik Diamond Creek Draft Major Activity Centre (Structure Plan)**

The [REDACTED] wish to provide feedback to Nillumbik Shire Council's Diamond Creek Draft Major Activity Centre (Structure Plan).

[REDACTED] request that a representative from the committee is part of the planning and implementation team in the roll out of the Diamond Creek Structure Plan.

Committee members have reviewed the draft Structure Plan and provide comment below.

[REDACTED] **support the following items within the Structure Plan**

- Support the rural village character feel of the surrounding township
- 56% agreed that the Activity Centre has a great atmosphere and environment. **This is something that needs to be addressed and provide more connection.**
- To create energy and vibrancy in the Activity Centre, changes to the existing land use pattern are proposed. **We support this only with proper consultation with traders and the community**
- opportunities within the Activity Centre are expected to relate to artisanal food production and hospitality
- introducing more lively activities in the Activity Centre, Diamond Creek will gain a more safe and vibrant environment both day and night.
- new opportunities for office space at ground or first floors of mixed-use development. **We support this only with proper consultation with traders and the community**
- The Activity Centre requires a generator of employment and investment
- Geografia suggests a key strategy may be to investigate the potential rezoning of Precincts 3 and 5 to a Commercial 3 Zone (C3Z), which would provide for a range of industrial, commercial, office and other employment generating uses which support the mixed-use function of the area. **We support this only with proper consultation with traders and the community**
- Further provision of cafes and restaurants would be highly complementary to residential development and the age and socio-economic profile of the local population.
- Precinct 4 to the south of the rail line is expected to continue its role as a retail location in its current form. With the intensification of sites within the activity centre, there will new opportunities for office space at ground or first floors of mixed-use development.
- Mixed use in Precincts 3 and 5 would breath life into the centre, allowing small businesses and creative industries to emerge and for Diamond Creek to become a hub of innovation. **We support this only with proper consultation with traders and the community**



- Moving the Shire Offices to Diamond Creek for example, would create a much needed major employment anchor, increase the viability of the shops and services, and bring vitality to the centre. To enable an employment anchor in the Diamond Creek Major Activity Centre, such as a civic or health anchor.
- improve access to and integrate retail, commercial and housing development with the railway station and its surrounds
- Provide the opportunity for a transit-oriented development (TOD) on land to the west of the railway line (Precinct 4) including uses such as commercial and residential. **We support this only with proper consultation with traders and the community**
- Undertake community consultation to review the potential application of C3Z (or a modified ACZ2) in the existing light industrial Precincts 3 and 5 to allow for a diversification of uses from creative industries, small manufactures, start-up businesses to offices, to facilitate business growth and employment opportunities. **Supportive of this**
- Encourage redevelopment at increased residential densities to provide additional dwellings within walking distance of the town centre. **We support this only with proper consultation with traders and the community, as the increase of traffic congestion could become an issue**
- Investigate implementation of indented parking on the western side of Elizabeth Street. **THIS IS A PRIORITY**
- Strengthen the landscape character along Main Hurstbridge Road through the provision of mature trees and planting within the streetscape and public realm. **THIS IS A PRIORITY**
- Investigate the opportunity for a public space/ plaza and the 'heart' of the Activity Centre to be located adjacent to the train station north of Main Hurstbridge Road away from the traffic congestion. **This is very important so that Diamond Creek is not seen as a thoroughfare but rather somewhere residents want to hang out**
- Prepare an Urban Design Framework (UDF) for Precinct 4 that incorporates a public space/ plaza and the 'heart' of the Activity Centre to the north of Main Hurstbridge Road away from traffic congestion.
- Enhance the public realm with seating, shade trees and space for outdoor eating.
- Investigate the provision of additional pedestrian railway crossings to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond.
- Investigate potential footpath upgrades and widening along Main Hurstbridge Road.
- Develop the retail, office and entertainment activities in and around Chute Street for local convenience shopping, a range of food and restaurant outlets and professional and other office based services but little if any retail floor-space expansion.
- Undertake community consultation to review the potential application of C3Z (or a modified ACZ2) in the existing light industrial Precincts 3 and 5 to allow for a diversification of uses from creative industries, small manufactures, start-up businesses to offices, to facilitate business growth and employment opportunities. **We support this only with proper consultation with traders and the community**
- Building heights across the activity centre should be no greater than 5 storeys, with 3 storeys adjacent to existing residential hinterland. • Tallest buildings (5 storeys) are clustered around the train station and within the Elizabeth Precinct, away from sensitive low-scale residential interfaces. **We do support some development in the area, but not at the sacrifice of losing the natural feel of Diamond creek and increased congestion of traffic**
- Liaise with the Department of Transport (DoT) to advocate for the removal of the level crossing and potential implementation of a sky rail as per the concept plan provided at Figure 8. **If this will reduce congestion in the long term it is supported.**

- Investigate the provision of additional pedestrian railway crossings to improve connectivity and legibility between the northern and southern sides of the activity centre and beyond. This is important to bring the whole centre together plus safety.

████████████████████ have concerns regarding the following items within the Structure Plan

- Less than half (46%) said it was easy to get to and get around the Activity Centre, this is very concerning. We need to look at bridging the gap within Diamond Creek
- Less than half (42%) like the look and feel of the Activity Centre. If people don't like the feel of an area they will have less connection, it is VERY IMPORTANT we build connection to build Diamond Creek
- Investigate the opportunity to make Station Street one way, to lessen conflicts between cars parking and pedestrians and reducing vehicular turning movements, minimising congestion. This is not supported at all
- Reduce the speed limit along Main Hurstbridge Road to 40km/h. This is not necessary, it is not a high accident zone and traffic doesn't travel fast anyway.

████████████████████ would like further clarification regarding the following items within the Structure Plan

- potential rezoning of Precincts 3 and 5 to a Commercial 3 Zone (C3Z), which would provide for a range of industrial, commercial, office and other employment generating uses which support the mixed-use function of the area.
- strategy is included that encourages redevelopment at increased residential densities to achieve an increase of 303 more dwellings within walking distance of the town centre by 2036.
- To focus retail activity in a compact core east of the railway, on the 'landscaped civic spine' of Main Hurstbridge Road, with a retail node in Chute Street.
- • Increased application of 5 storeys adjacent to the train station in Precinct 4 to capitalise on the high level of access to public transport and lack of sensitive interfaces; • Increased application of 5 storeys in Precinct 3 due to the lack of sensitive interfaces; and • Increased application of 4 storeys in Precinct 1 to capitalise on creek frontage.
- Reduce the speed limit along Main Hurstbridge Road to 40km/h.
- Encourage higher density development in and around the Chute Street node.
- Investigate the implementation of on-road cycling lanes on Chute Street
- Advocate for reduced speed limits in Main Hurstbridge Road
- Review zoning and consider application of a Commercial 3 Zone (C3Z) (or modified AC22) to facilitate creative industries, small manufacturers, start-up businesses and residential.

Kind Regards

██████████

████████████████████

**From:** [Julie Paget](#)  
**To:** [Strategic Planning](#)  
**Subject:** FW: Individual Submission to Diamond Creek Major Activities Centre Structure Plan - Draft GILA  
**Date:** Monday, 4 May 2020 3:43:00 PM

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**From:** [REDACTED]  
**Sent:** Wednesday, 29 April 2020 5:01 PM

[REDACTED]  
[REDACTED]  
**Subject:** Individual Submission to Diamond Creek Major Activities Centre Structure Plan - Draft

29th April, 2020

Individual Submissions to Diamond Creek Major Activities  
Centre Structure Plan – Draft

Dear Rosa

See attached individual Submission to Activity Centre  
Review.

Thanking you

[REDACTED]

I or Nominees wish to be heard on our objections suggestions by Council. We request an Independent Panel to hear our Submissions if they cannot be incorporated by Council. We reserve all our rights. This includes further submission once we have received copies of all relevant studies that informs this draft (including those related to provision of reticulated infrastructure, to provide for draft.

We submit one respect Diamond Creek true heritage by :

1. Extend the Activity Centre boundary to include all of the original Diamond Creek township. This includes Diamond Street, Bage Street, Collins Street.
2. Ensure proper signage indicates all these above gateways as the beginning of Diamond Creek Activity Centre.



3. Rename Precinct One as Retail Core 1. Remove description as a Commercial Strip.
4. Encourage all Retail, Commercial, Business, Employment opportunities and other normally afforded to a township to progress to their highest and best use in line with free enterprise.
5. Remove all references, policies and strategies that designate Precinct One, including (Chute Street) as a secondary node for any use and development, or to another node to another Precinct of Diamond Creek.
6. Include bus transportation from all above gateways mentioned, to include Chute Street to Train Station. Add new traffic lights and Pedestrian crossing at ground level at the Corner of Chute Street and Diamond Street and from the Train Station crossing over to Chute Street for safety.
7. Reduce two to three height limits to three storey or less without a mandatory minimum.
8. Ensure that no new strategic policies or strategies are put in place that disadvantage Precinct One Housing opportunities and those of its Plan, Paid for, Establish planned growth north of it which includes the acreage properties of this Submission.
9. This may require Council's request for Yarra Valley Water to delay their current infrastructure works for the City of Whittlesea, that may be used for infrastructure planned and established for this area.
10. Submission to be added to following readdressing of consultation concerns and needed information.

Thank you

[REDACTED]

[REDACTED]

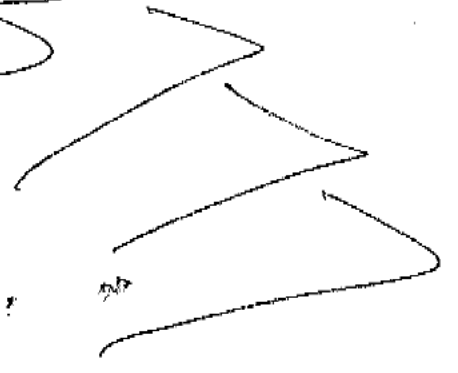
[REDACTED]

[REDACTED]



To 94333

William  
Stine  
Cowan



Page 010  
To Fax [redacted]

URGENT

Attention

Rosa Zoolas  
and Nillumbik Shire Council

29/4/2020

Submission [redacted]

D.ck Major Activity Centre Draft Structure Plan - rushed no time to type

STRONG OBJECTION to flawed, unbalanced and inequitable study area and sub-precincts that makes a new D.CREEK suburb totally unrecognizable.

- 1. I request to be heard by Nillumbik Shire Council
- 2. I request to be heard by Independent Panel of State Government
- 3. I re submit all my submissions since commencement of new D.Ck urban Growth Review - which left out proper established D.Ck chute st township and established surrounding roads planned for growth. They were all tabled and included submissions to the more recent Independent Panel by State government on the D.Ck Major Activity Centre topic.

4. Why has Council totally ignored recommendations of Panel and again commenced new Goal posts

5. Demoting original D.Ck chute st side? The panel recommended one Activity Centre - EQUAL with importance

6. RESPECT THIS. RESPECT PROPER D.Ck side linking Greensborough and Northern Corridor (and long term investors) 7. DON'T DIVIDE DIAMOND CREEK  
8. UP HOLD and PROTECT HISTORICAL STRATEGIC GOALS

Page two

- 9) Please investigate if infrastructure has already been redirected in any way that fits in with your new study area and precincts which in effect may freeze permanently areas that were planned to progress from historical establishment side, including roads such as Ironbank.
- 10) The new study of Diamond Creek Urban Growth Review - incorporated AB (at Daref) and LEFT OUT All of already established Residential Why has this been cemented instead of correction? (as it appears with this new draft Major activity Centre)
- 11) Why have you disproportionately shown such large areas of Montsbridge road to progress further (ie East Diamond Creek or Wattle Glen) and left out completely the North North West and everywhere else from side Chote st up? (Proper suburb/township of Diamond Creek)
- 12) Look after your investors - original side of Diamond Creek township with equity - particularly long term investors with well founded expectations. Don't expect these people to pay for Green wedge aspirations by (freezing) their lands by YOUR NEW STRATEGIC study area and pre-precincts will Demote their lands and destroy any future structure plan and accompanying new strategic goal posts that will enable new financial gains at the expense/demise of the old landowners with well founded vision (and payments)
- 13) I request (disbandment) of new draft structure plan and accompanying new strategic goal posts that will enable new financial gains at the expense/demise of the old landowners with well founded vision (and payments)

20/04/2020

(14) Where were the representatives of Chote St side of township with the 'new Design Guidelines' committee?

It needs to be 'reformed' with equal representation and then you will have a more balanced 'major activity centre draft structure plan.' (Your map will then reflect it)

(15) Precincts too over development East side of Diamond Creek - destroys character of Diamond Creek and a total contradiction to G.W. ideals.

- it would take up too much urban infrastructure as would need high capacity and flows to reach multi-levels

(16) High level precincts ~~in~~ East side of Dick has serious impacts on any form of development expected and planned for on surrounding acreages from plenty growth side of original Chote St town centre.

(17) Demand for large backyards, acreages half to one acre is more popular than ever now and landowners have been waiting long enough to provide it - so release it with precincts coming from Chote St where it was meant to.

(and without excuses!)

boxes to new Diamond Creek East or is it Wattle Glen?!

(18) Infrastructure is the crux of all planning and you have made a map where new infrastructure has to be provided. Where is the sense of that? Or has it gone there before this planning process has been completed? DO NOT IMPACT EXISTING ESTABLISHED AND PAID FOR INFRASTRUCTURE. RELEASE THIS FIRST AND REFLECT THIS WITH YOUR MAPPING!

Page for

19) Is there data available for the public to see infrastructure capacity of all utilities, in Diamond Creek and surrounding roads?

Infrastructure is costly and takes years and years of planning, capital works and ongoing costs and charges including relevant urban municipal and infrastructure rates and thus should be the pivotal cross of your strategic goals now with structure plan — not new goal posts and new benefactors!

Allow established areas to go ahead now and your new plans for the future — so please adjust your mapping!  
(Duty of care is expected by long term investors)

20) Your new plan — demotes/prejudices/impacts seriously old timers (investors especially for new housing strategy that will inter-relate) (land holders)  
Please

21) Uphold previous Diamond Valley Planning (associated certainty for landholders) together with Plenty Growth and Water Trust goals with your mapping of growth now for Diamond Creek and add your new precincts to complement and unite the suburb of Diamond Creek without dividing and destroying the (originals)?

22) Uphold previous Independent Panel recommendations of Diamond Creek — one activity centre — equal importance.  
Regards





## **Submission:**

### **Diamond Creek Activity Centre – Draft Structure Plan**

We are glad to have the opportunity to provide feedback on the Draft Diamond Creek Activity Centre Structure Plan (the draft). The Activity Centre is important not only for those within the 3089 suburb, but also for users from outside of the Shire and residents in small towns in the more rural parts of the Shire, particularly to the north of Diamond Creek, for whom Diamond Creek is often their ‘local’ place for weekly grocery shopping, sports, and other activities.

#### **General comments**

The draft is generally a very good document – it is comprehensive in the coverage of the parts that make up the Activity Centre, and easily understandable in explaining what it is trying to achieve.

There are many ideas within the draft that are worthy of praise, and that align with the community feedback Council has received and the goals of creating a town that is ‘environmentally, socially and economically sound’. [pg 4].

As the draft highlights, community engagement revealed that residents ‘felt there was a good sense of community in Diamond Creek’, ‘loved the open space, trees and sporting facilities’ but also believed there was a ‘poor’ atmosphere due in part to accessibility and transport issues. Respondents to community engagement also raised specific concerns about improving the train station and shopping areas, as well as leisure and recreational spaces. It is these sentiments that need to be the focus for Council’s approach to the plan and its implementation.

Below we outline our feedback on the vision and each of the five themes used in the draft, focusing on areas of improvement and concerns we hold.

## **Vision**

██████████ largely agrees with the proposed vision for the Activity Centre, and the aspirations within it for a town that celebrates and enhances its semi-rural setting, mixed uses, and environmental qualities such as the ‘strong presence of native canopy trees’.

Any development that takes place in the Activity Centre should be of a high quality and be sustainable – both in an environmental sense and with regard to managing pressures such as congestion and accessibility. To further emphasise this point we suggest that the words ‘and enhance neighbourhood character’ be added to the second paragraph, following ‘...pay homage to its landscape setting and history.’

While we do not disagree with the aim that Diamond Creek ‘build its reputation as a boutique and artisanal food production destination’ we do note that the draft plan for the Eltham Activity Centre expresses an almost identical sentiment. Diamond Creek and Eltham are both unique towns and it is important that each town continues to have its own distinctive identity.

## **Land Use Activities and Development**

This is a critical aspect of the draft. ██████████ agrees that there are opportunities to improve parts of the Activity Centre to provide a better atmosphere in the town. Local employment should be encouraged through businesses providing a point of difference to larger retail centres.

The discussion around the future uses of Precincts 3 and 5 is interesting. Mitre 10 in Precinct 5 is an important local business that is well patronised by the community and employs a substantial number of people. The businesses in Precinct 3 are diverse, including automotive and trades businesses, and play a key part in contributing to the range of services in the Activity Centre. Diamond Creek needs to retain this variety of businesses, including ‘industrial’ types, to meet the expectations and needs of a town of this size.

██████████ recognises that extending the range of uses in these two precincts would allow flexibility. Of the two options proposed to achieve this – potential rezoning of Precincts 3 and 5 to Commercial 3 Zone (C3Z), or modification of the existing Activity Centre Zone 2 (ACZ2) – we believe that the best way to achieve the intended result is pursuing the modification of ACZ2. This will ensure that neighbourhood character is kept at the forefront of any considerations for developments within the precincts.

The Eltham and Diamond Creek Activity Centre drafts both highlight the importance of a ‘large employment anchor’, suggesting in both towns either a health or civic type development to boost local jobs.

The Diamond Creek draft pinpoints Precinct 5 – perhaps eyeing the large area of land currently occupied by Panorama Coaches – as an option for the relocation of the Council

offices from their current location. Unless Council plans on employing more staff, this relocation would not increase the number of local jobs in the Shire, but it is recognised that adding a major employer in the Activity Centre could provide a boost to the nearby retail core in Precinct 4. Such a re-location would also put the Council offices within reach of a substantial public transport hub thereby obviating the need for residents to drive to the offices quite so often.

However, instead of the council offices, Council should focus on the health sector suggestion. Alternatively for an 'anchor' Council could consider the multi-use community hub that has otherwise been suggested for the existing Diamond Creek Community Centre, which is a site that cannot fit all of the services the community would require.

██████████ recognises the value of focusing any higher density development in the 'core' of the Activity Centre, being close to the retail precinct, community facilities and public transport links. These developments must meet the highest standards of responsible and sustainable building, as well as contributing to an enhanced neighbourhood character.

██████████ is supportive of the objectives to cluster community facilities in the Activity Centre, focus retail activity in Precinct 4 alongside the Chute St group of shops in Precinct 1, and provide additional housing within the Activity Centre predominantly in Precincts 4 and 6. These are all sensible aims.

### **Built Form Character**

Heritage is an important aspect of Diamond Creek and is part of what makes the Town unique. We agree with the draft that the various heritage buildings must be preserved, as should the significant canopy trees throughout the Activity Centre.

The proposals to alter maximum height limits, largely based in the immediate area on either side of the train station, would be a significant change for Diamond Creek. Whilst we understand an increase from three to four storeys (and in some cases five storeys) in the broader context of Melbourne's population projections for the next ten years, and agree activity centres are the right places for this growth to occur, we do find this proposed increase surprising. We accept that some development will need to occur but point out that Diamond Creek is an activity centre on Melbourne's fringe and should not be treated in the same way as other activity centres closer to the CBD.

Multiple-storey developments should be focused in Precinct 4 where there is easy and attractive access to amenities. The suggestion that Precinct 3 could have developments of up to five storeys could be seen as placing an endpoint on the existing businesses occupying the precinct.

We agree that the current layout of parking, particularly in Precinct 4, contributes to a 'poor pedestrian environment' and does not encourage people travelling through the Activity

Centre to patronise the retail and hospitality options on offer. While the suggestion of moving parking to the rear of properties or basements is not possible everywhere in the Activity Centre, it certainly should be implemented where appropriate, and where it meets community standards.

██████████ is supportive of the suggested setbacks in this section and the recognition that developments must avoid impacting adjacent properties with issues such as overshadowing. We encourage further emphasis on the value of landscaping to soften the impact of buildings and contribute to the green, semi-rural character of the town, and note that the Victorian Government's Better Apartments Design Standards recommend canopy trees where possible.

Both the original Activity Centre Structure Plan and the draft talk about building a unique character for Diamond Creek, and we agree that a local architectural style for Diamond Creek reflecting the natural setting and neighbourhood character is important. As the draft suggests, development in the Activity Centre needs to complement the landscape, hilly topography and vegetation.

### **Movement and Access**

██████████ is supportive of the identified need for improvements to footpaths, pedestrian crossings, on road cycle lanes, bike parking, and generally better connectivity throughout the Activity Centre. As has been highlighted elsewhere in the draft, more needs to be done to encourage better connections across Main Hurstbridge Road (MHR) and the train line.

Similarly, the reduction of the speed limit to 40km/h along MHR in the Activity Centre – in line with other activity centres such as Eltham – would be a welcome development by the community, particularly considering calls for this change in recent years.

Installing safe and dedicated spaces for pedestrians and cyclists, and connecting these with existing recreational trails, will undoubtedly encourage more people to utilise these forms of movement rather than motor vehicles.

We are alarmed at the enthusiasm in the draft for the installation of a so-called 'sky rail' to replace the existing level crossing at MHR. While we agree that the level crossing restricts connections between the two sides of the railway line, and contributes to traffic congestion,

██████████ does not believe a 'sky rail' is the answer. Such a proposal is at odds with the semi-rural character that is celebrated in the draft, and we believe at odds with what the community would support. Council needs to consider alternative approaches to addressing the level crossing, including further work on the viability of an underpass.

### **Landscape and Place Activation**

As the draft notes the local community places high value on the ‘natural landscape and low-scale buildings’ in the Activity Centre, and any development that occurs needs to complement the landscape. We agree with the recognised need for the increased presence of vegetation – preferably native vegetation – to provide a positive contribution to the local environment and feel of the Activity Centre. Canopy trees are especially important in urban settings because they offset heat island effects that are likely to become an even greater issue with climate change.

██████████ applauds the focus on the need for a ‘public plaza’, a community gathering space for Diamond Creek. The 2006 plan for Diamond Creek had similarly raised the need for such a development to create a focal point for the Activity Centre. As both the original plan and this draft suggest, the most appropriate place for this is in Precinct 4. We encourage Council to prioritise a comprehensive community consultation process on this proposal for its 2020-2024 term.

We agree that MHR, along with the railway line, poses the biggest challenge to creating an accessible and central gathering space, but also acknowledge that the Precinct 4 retail existing on both sides of MHR provide a range of services that meet the varying needs of residents. Retail on both sides of the road should continue to be supported and activated to ensure its long-term future.

### **Community and Leisure**

██████████ agrees with the identification that an ‘obvious deficit’ in the Activity Centre is a multi-purpose community building. As noted in the draft, the Diamond Creek Community Centre – while greatly valued by the community – largely serves as a leisure and fitness centre, rather than a community hub.

A multi-use facility in Diamond Creek could provide some of the existing uses of the Community Centre, and also a dedicated space for youth (not just youth services but also ‘calm’ spaces), health services, and meeting rooms.

Considering the number of community groups and volunteers in the Shire, there is a general shortage of local affordable and accessible event spaces and meeting places; a community hub in Diamond Creek should play a role in addressing this issue.

Spaces that can be rearranged depending on the activity should be a priority of this multi-use building; there are a number of examples of this around Melbourne, including the flexible use space at the Footscray Community Arts Centre.

A library in Diamond Creek has been proposed for many years. If there were to be a new library built Diamond Creek would appear to be the most sensible place to locate it, and it would make sense to co-locate it with a new multi-use building.



Considering its importance, there must be stand-alone community consultation on a proposed new multi-use building. To maximise support for the project residents must feel ownership of it and feel that they have been 'brought along' in the process from the beginning.

Council needs to be mindful of the location and visibility of such a building. While we think nearby equivalents such as the Hurstbridge Community Hub are excellent facilities that serve as good models, buildings of this kind deserve (and need) to be located in a prominent position – in Diamond Creek's case this means along MHR.

Council also needs to avoid 'packing in' too much into one building. We are not sure that the existing site of the Diamond Creek Community Centre can accommodate additional activities on top of what it already provides, which is why we would suggest that Council consider a new stand-alone facility – meaning that with the existing Community Centre site and a new site, one would be a 'fitness and recreation' centre and the other a 'community hub'. This new building could perhaps be located in Precinct 5, to connect to the retail core in Precinct 4.

██████████ agrees that advocacy for improvements at the Diamond Creek train station is important, including platform upgrades, accessibility, and the general look of the station.

## **Precincts**

The bulk of our feedback concerning the precincts is including above in the discussion of each 'theme', but we also note below additional points that directly relate to the individual precincts.

### *Precinct 1*

'Higher density development' is better focused closer to the middle of the Activity Centre, around Precinct 4. Chute St has its own history and special place in providing retail and other services and any development should respect the scale of the existing precinct and topography as it leads down to the creek.

### *Precinct 2*

Apart from existing exotic species in the Town, such as the elm tree avenue at Marngrook Oval, ██████████ encourages the prioritisation of new planting and landscaping being comprised of native species. The suggestion of an underpass to connect the Diamond Creek Trail over the MHR needs further focused community consultation – we wonder if a pedestrian crossing could not serve much the same outcome in this situation.

### *Precinct 4*

We note that the suggested commercial and/or residential 'transit-oriented development' identified for VicTrack land 'west of the railway line' seemingly would see the commuter car

park for the train station removed. If Council wanted to proceed with this suggestion it would need to elaborate on what ideas it would have for how a development interacts with the train station and accessibility to the station, including for buses. Council should also clarify the map at Figure 16 which appears to show a suggested realignment (and perhaps widening) of MHR.

*Precinct 6*

██████████ does not think ‘more intensive development’ to make the use of land more ‘efficient’ needs to be encouraged in this part of the Activity Centre. Development should reflect the scale of the existing buildings, noting that most are one storey residential or former residential buildings.

**Conclusion**

The draft is on the whole a very good document; it sets out an ambitious plan for a more sustainable and connected Diamond Creek, and importantly, provides a roadmap for how to achieve this.

██████████ urges the Council to keep its focus on what changes can be made to address the community sentiments and feedback received to date. Residents from Diamond Creek and further afield want to build a stronger sense of community, and this should be the overarching ideal guiding Council’s approach to the Diamond Creek Activity Centre over the next ten years.

████████████████████  
████████████████████

April 2020

To Julie Padget  
Strategic planning  
Nillumbik Council  
26 April 2020

Resident  
[REDACTED]  
Diamond Creek

## **Attention meeting of Annual Draft Budget 2020-2021**

RE-Draft Structure Plans for Diamond Creek Activity Centres No.5 and No 6.

You call yourself the GREEN WEDGE SHIRE ! I was absolutely horrified when I saw and read about the proposed building plans( especially no.5 and no.6 ) for 4 to 5 storey buildings and apartments along Main Hurstbridge Road Diamond Creek.

NO NO NO to any buildings above two levels in this part of Diamond Creek. !!

Firstly, This main road is already dealing with quite a heavy flow of traffic a lot of the time and would be chaotic with having to deal with even more traffic ! Further large developments will only make this road even worse and more dangerous for school children and pedestrians alike.

Will increase noise levels in the area, which is currently low and a pleasant area to live in ,in Diamond Creek.

More traffic will be pushed onto using side streets thus ruining what is now a pleasant area to live in ,and will be more dangerous for children and the elderly who live in these side streets. Two storey buildings are bad enough, 3,4 and 5 storey buildings should not be built in this area or anywhere in Diamond Creek.!!

Large buildings;

Loss of privacy for existing residences having buildings towering over their back and front yards.

More people living in proposed apartments, meaning even more cars, more noise, more congested roads, etc.

I choose to live in [REDACTED] Street, a sort after street to live in for it's location, on the edge of the shops and for it's quietness and view of the near-by bushland. A relatively peaceful place to live in .If I didn't mind multi storey buildings, I wouldn't be living in Diamond Creek.!!

If the proposed buildings go ahead, and of 3 or more levels, this part of Diamond Creek will be ruined as well as the whole of Diamond Creek.

\*\*\* DO ANY OF YOU COUNCILLORS LIVE IN OR NEAR THE AREA WHERE THESE PROPOSED BUILDINGS ARE GOING TO BE BUILT ?? Probably not. So please put yourselves in our position (If you can) and see how you would feel if a 3-5 storey building/eyesaw, was built next door to you or across the road.

PLEASE DO NOT GRANT PERMISSION FOR THESE 3-5 STOREY BUILDINGS/ACTIVITY CENTRES TO BE BUILT IN THIS AREA OR ANYWHERE IN DIAMOND CREEK.

An absolute two storey limit on any buildings, any more is not needed.

Please don't ruin Diamond Creek. It's attraction is it's country feel/atmosphere and a quiet township. Think about the name of our shire – GREEN WEDGE SHIRE . Stand true to this name and it's meaning. This name certainly doesn't speak or support building multi level buildings. Go to the inner suburbs for this kind of living !!

Again. Please do not grant permission for the mentioned proposed building plans to go ahead.

Think about the meaning of GREEN WEDGE and not about the millions of dollars to be made out of such developments and in the process, losing Diamond Creek's identity. Money can still be made on smaller scale projects.!

I hope you read and seriously consider what I have said in my letter.

THANKYOU

*Yours Sincerely*  
*ANON*

*Resident in*  *Street.*



## Department of Transport

Metro Operations  
12 Clarke Street  
Sunshine Victoria 3020

### Mr Paul Fyffe

Senior Strategic Planner  
Strategic Planning  
Nillumbik Shire Council  
*Paul.Fyffe@nillumbik.vic.gov.au*

Dear Mr Fyffe

### **Eltham and Diamond Creek Activity Centre Structure Plans.**

Thank you for the opportunity to comment on the Eltham and Diamond Creek Activity Centre Structure Plans.

With reference to the Department of Transport's (DoT) earlier correspondence dated 12 July 2019 (copy attached), it is reiterated that DoT, in principle, supports Councils' planning approach. DoT fully supports Councils' objectives to improve the provision of public and active transport, including improving pedestrian and cycling connections between rail stations, bus interchanges and the wider precinct.

DoT is committed to working with Council to consider transport improvements, particularly those interfacing State infrastructure, that will achieve the best possible outcomes for all users while balancing the needs of access to activity centres and broader network movements.

On this basis, DoT welcomes further discussion with Council as it seeks to further develop and finalise the structure plans. Notably, any part of the structure plans which may have an impact on the reliability or capacity of the transport network requires a degree of analysis for DoT to consider. Further, any analysis should seek to identify the cumulative benefit and impact of transport elements within a reasonable vicinity. This includes consideration of existing and proposed interfaces, such as the implications of proposing a new signalised intersection within close vicinity of an existing signalised intersection(s).

Where not agreed, the structure plan should clearly identify transport elements that are subject to further consideration. Where possible, for key transport elements required to support the structure plan, consideration for the timing and likely funding sources should be identified.

Specific issues for review and further consideration by Council for the Eltham and Diamond Creek Activity Centres are as follows:



## Eltham

1. Bolton Street is now a declared arterial road and should be recognised as DZ1 for the purposes of the structure plan. Bolton Street's future function as an arterial road needs to be clearly articulated in the structure plan.
2. Any proposals for changes to the arterial roads of Bridge Street, Bolton Street and Main Road that impact their arterial function will require analysis and review agreement by DoT prior to finalisation of the Structure Plan.
  - a) This includes proposals for pedestrian access upgrades such as pedestrian operated signals.
3. The Diamond Creek Regional Trail Link congestion issue on Main Road through the Eltham Activity Centre is noted. DoT has indicated to Council its support for the establishment of a Strategic Cycle Corridor (SCC) that would be aligned west of the rail line with appropriate upgrade to supporting secondary shared path links to the activity centre. It is suggested this proposal could be included in the Structure Plan.
4. Figure 8 shows:
  - a) proposed Gateway treatments for Sherbourne Road, Bolton Street, Bridge Street and Main Road and landscape boulevard treatment on Main Road.
    - These treatments must be designed commensurate with the function of the arterial road network.
5. Figure 11 requires clarification regarding scope of infrastructure envisaged for "improved connection" to the station. Cycling facilities for Main Road should be noted in the Structure Plan as being subject to further collaboration between Council and DoT in the context of the development of the Strategic Cycling Corridors (SCC) network and its links.
6. Figure 12 shows:
  - a) Main Road crossings/POS/Signals from Arthurs Street to Cecil Street.
    - This density of signalisation or other controls is not supported in principle.
  - b) Luck Street to be examined as a future through road with an unsignalised intersection with Main Road.
    - DoT recommends Council considers the engineering limitations of delivering such a link due to grades. The intersection control type may also require review; the demand for such crossing is unclear.
7. Figure 13 shows:
  - a) A bridge over the rail line.
    - Clarification is required whether it is intended that the bridge would extend to cross Main Street. It is suggested that there is some merit in considering such a crossing.
8. Figure 20:

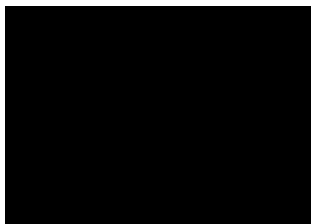
- a) Clarification of the impacts or implications of the improved public amenities on Bolton street is required.

Diamond Creek

1. Figure 7 shows:
  - a) a proposal for a raised pedestrian crossing on Hurstbridge Road.
    - a. This proposed treatment is inconsistent with the function of an arterial road. DoT seeks clarification regarding whether Pedestrian Operated Signals are included;
  - b) Introduction of flashing lights.
    - a. Clarification required as to details of treatment proposed.
  - c) On Road Bicycle Lanes.
    - a. This requires further consideration of an off-road path to increase safety and promote cycling as a transport mode for all user groups. As such, cycling facilities for Main Street-Hurstbridge Road would benefit from further collaboration between Council and DoT in the context of the development of the SCC network and its links.
2. Figure 7 should show, or a new figure be introduced to show, all existing pedestrian crossings and signals overlaid with the Structure Plan's proposed transport initiatives so these transport elements are not considered in isolation of one another. If Council identifies desire lines or demand for new pedestrian facilities, there should be a clear understanding of the deficiencies of existing infrastructure and where agreed the structure plan should show removal of existing facilities in favor of new crossings as agreed between DoT and Council.

For further enquiries or clarification regarding the above comments or information referenced in DoTs' letter of 12 July, 2019, please contact [REDACTED], Senior Transport Planner at the Department of Transport's Metro North West Region; by email [REDACTED]

Yours sincerely



Manager  
Network Planning Integration  
Metro North West Region

26 June 2020

Submitter 43

Rosa Zouzoulas  
Nillumbik Shire Council  
PO Box 476 Greensborough VIC 3088

Dear Rosa,

**Proposal:** Pre-development advice

**Site location:** Diamond Creek Major Activity Centre (Draft structure plan)

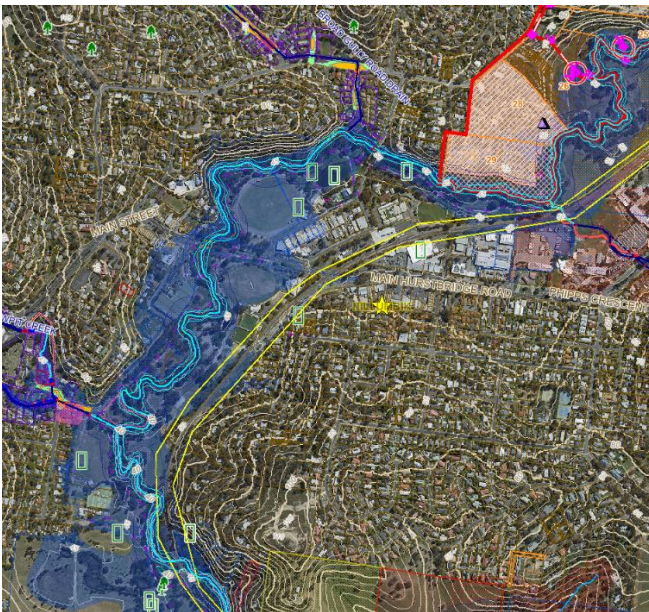
**Melbourne Water reference:** MWA-1168165

**Date referred:** 10/03/2020

Thank you for your application for Melbourne Water's advice on the Activity Centre Draft Structure Plan. It is noted the current draft Structure Plan referred to Melbourne Water is a review of an already existing Plan (2006) with council.

### Relevant Context & Policy

Given the presence of high flood hazard areas within the structure plan, Melbourne Water recommends that Council give consideration as to how the plan can support the objectives of Clause 13.03-1S Floodplain Management in the Nillumbik Planning Scheme. In particular, it is recommended that the Plan consider how the strategy of Clause 13.03-1S to 'avoid intensifying the impact of flooding through inappropriately located use and development' can be achieved in this area.



**Figure 1: Flood extent associated with the waterway (blue flood shape) and underground drains (purple flood shape)**



The area under Diamond Creek Activity Centre is located within Melbourne Water's region of waterway and drainage responsibility. Under the legislation (P & E Act 1987, Water Act 1989 (Vic)) Melbourne Water has a number of broad functions and obligations which include to:

- provide advice about flooding and controls on development to local councils;
- protect and enhance drainage systems and waterways;
- implement programs for the protection and enhancement of in-stream uses; and,
- improve water quality in drainage systems.

### **Flooding information**

Melbourne Water assesses development applications in accordance with our adopted Guidelines for Development in Flood Affected Areas (DELWP) (Guidelines). Under these guidelines, development in or adjacent to a floodplain may only be acceptable where the new development is protected from flooding, has safe access to and around the development and does not interfere with the passage and storage of floodwaters.

The Guidelines note that flood risk can be limited by encouraging development intensification away from areas of high hazard. The standards set out in the 'Chapter 8: Flood Safety' of the Guidelines outline what is considered to be acceptable thresholds of flood hazard for different development types. For most types of development intensification, the maximum flood depth criteria ranges from 300-500mm. (Refer Guidelines attached)

Melbourne Water's review of the structure plan has indicated that some areas identified for development intensification are within areas of high flood depths and therefore future development within these areas will be of concern to Melbourne Water. (At permit stages) Melbourne Water recommends that the structure plan outline the areas of high flood hazard and if possible direct development intensification away from these areas. (Flood mitigation works could also be considered)

### Site specific feedback

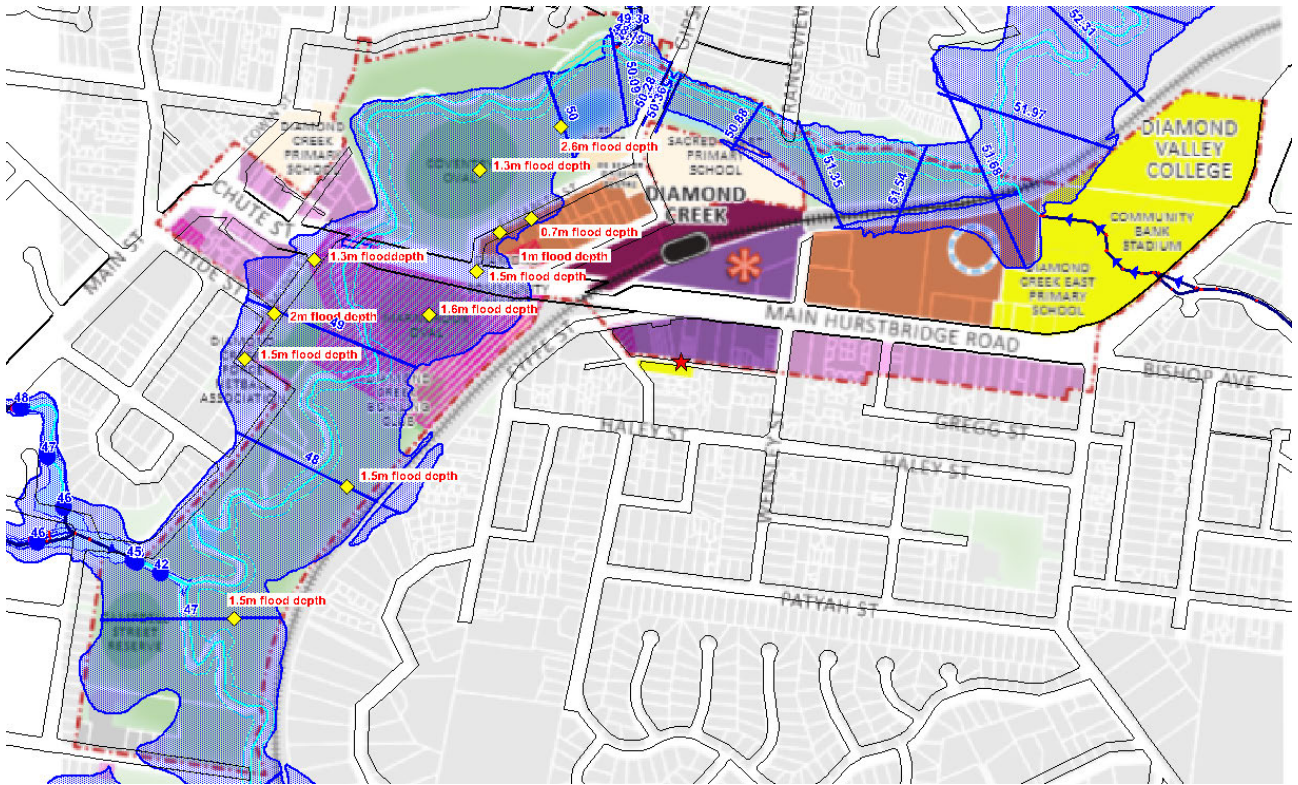
- In particular high density residential & commercial developments (in vertical built form) shown at Diamond Street (adjacent to Chute and Hyden Street) and Elizabeth Street and Main Hurstbridge Road, where the 1%AEP flood depth varies from 0.7m deep to 2m deep (*Refer Fig: 2*) must be considered at their formulation stages as per Melbourne Water's guidelines/objectives for development within flood prone land, that is, Site and Access safety, Flood damage and Off-site Impacts.

In addition to above, following measures are to be considered:

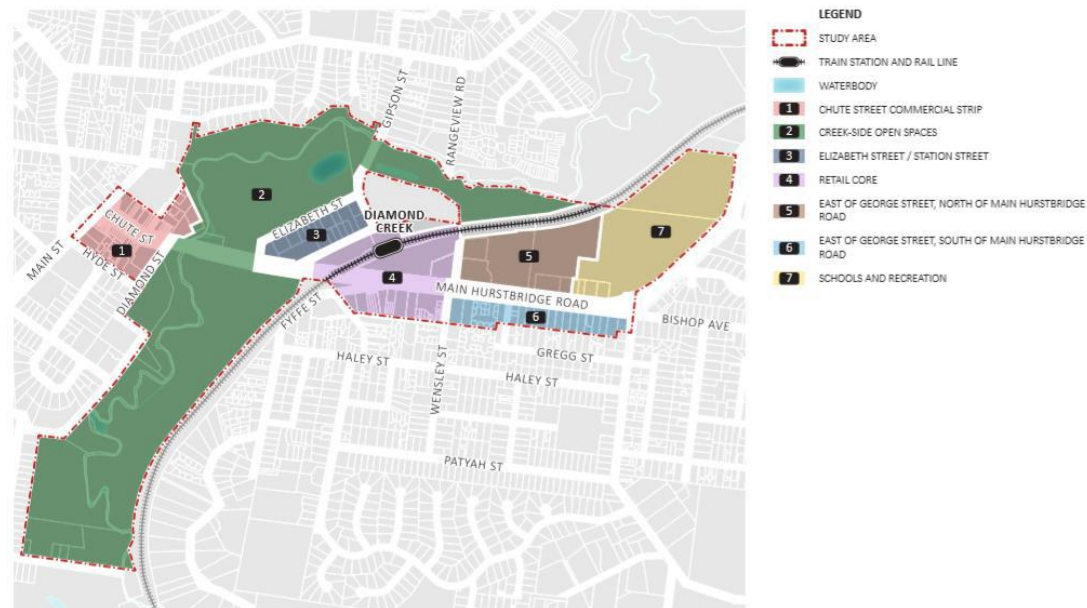
- Due to limited information on flooding issue in this area, further hydraulic investigation will be required to assess the potential impact on existing drainage system. Melbourne Water requires no increase in flood levels by any future development;
- Developments within large vacant land/ properties are required to retard development flows back to existing condition flows;
- Existing 1%AEP flood storage of the Diamond Creek must be maintained to avoid downstream flooding;
- Climate change. Consider increase in rainfall intensity for future potential development;
- Shared paths and pedestrian bridges are required to be built in accordance with Melbourne Water's Shared pathway Guidelines;



- Bridge widenings or duplications on the Diamond Creek must be designed to convey 1%AEP flows without increase in flood level. A 600mm freeboard is applicable to all new bridges;
- Community facilities (if any) (Stadiums, Pavilions, Community Centres) built within and above 0.5m deep flooding will be concerning to Melbourne Water;
- Melbourne Water’s Parkway underground drain runs through community bank stadium and drains onto Diamond Creek. The %AEP gap must be conveyed through this area without increasing 1%AEP flood levels.
- All properties must be developed in accordance with DELWP’s “Guidelines for Development in Flood Affected Areas” of 2019.



**Figure 2: 1%AEP flood extent and flood depth**



**Figure 3 Precincts**



## **Waterway health information and comments**

- Stormwater management should be considered as part of the activity plan. Melbourne Water is supportive that as part of the built form character, there is an objective to promote Water Sensitive Urban Design. Considerations should be made to include a strategic document to inform an Integrated Water Management plan for the precincts.
- Litter is an issue within the Diamond Creek. Platypus entanglement in litter a significant concern within the Diamond Creek. Litter is also unsightly and reduces the enjoy ability of the creek for users. The source of the litter is in many cases from the Activity Centre. Strategies to reduce the source of the litter from activity centre precincts would assist Melbourne Water in protecting and enhancing the Diamond Creek.
- The Healthy Waterways Strategy 2018 has a performance objective of maintaining and increasing the vegetated buffer of the Diamond Creek to 20m. This would help achieve a healthy waterway and to protect key aquatic values known to be present such as Platypus and native fish. To achieve this, there should be no new infrastructure within 20m of the top of bank of either side of the creek. A collaborative approach of management of the creek should be established to meet this objective. Any development adjacent to the creek must be approved by Melbourne Water.
- Given the Diamond Creek is nominated as part of the green spine of open space and parklands along the creek. Melbourne Water welcomes suggestions to improve and enhance the management and vegetation along the creek corridor. Melbourne Water currently undertakes capital and maintenance works along the creek, having completed three capital projects in the precinct area in the past 5 years. Opportunities to work with Council on the implementation of this plan to improve the amenity and environment of the creek are welcomed.

## **Melbourne Water's Strategy Objectives relating to waterway health**

### *5.1 Precinct 1: Chute Street Commercial Strip*

- To encourage buildings to step down the slope providing a link and orientation to the natural settings and leisure opportunities along the creek valley;
- Development on lots directly interfacing with the creek should be designed to address the creek with entries, windows and balconies.

Melbourne Water would be supportive of improving connections to the creek as long as the proposal is sympathetic to the Creek's natural landscape. Current housing, especially at Inglis St imposes on the creek corridor and reduces the natural landscape values. Increasing setbacks, screening vegetation, natural colours and finishes would improve the current visual impact. Flood risk must also be considered if increasing development in or near the creek.

### *5.2 Precinct 2: Creek-side Open Spaces*

- To ensure the extent and nature of development should be limited and directly related to parkland based activities and promote use of the public parkland space

Melbourne Water has spent considerable capital and maintenance funds to enhance the quality of the creek. We will continue to enhance the waterway to meet these strategies and our own Healthy Waterways Strategy 2018. Objectives listed for the precinct should include:

- Continue to enhance the quality of the Diamond Creek.
- Undertake additional planting in the parkland adjacent to the Diamond Creek

Melbourne Water supports improving trail connectivity along the creek for social objectives. Any upgrades to park infrastructure must be sensitive to the creek and corridor, including maintaining where possible a 20m setback.

### **Asset Protection Information**

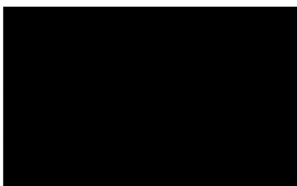
- All precautions must be taken to prevent any impact or damage to Melbourne Water's above mentioned assets which are within or bordering the Activity Centre Draft Structure Plan area.
- Any works or developments within a 5m distance of any of Melbourne Water's assets will be subject to specific asset protection requirements/ conditions from Melbourne Water. (Build over approval may be required through a separate on-line application process)

### **General Advice**

This advice is valid for a period of three months from the date of this letter.

The above information is only preliminary and forms no contractual agreement between your company and Melbourne Water. Melbourne Water reserves the right to alter any or all of this information at any time.

If you have any enquiries, please contact me on 131 722 or email [devconnect@melbournewater.com.au](mailto:devconnect@melbournewater.com.au), quoting Melbourne Water's reference number in the subject line.



Indi Prathapasinghe  
Development Planning Services

27<sup>th</sup> April 2020

██████████ PTY LTD Submission to the Draft Diamond Creek Activity Centre Review.

Again in the interests of fairness and in the current climate we ask that Council *delay this process* as it has not been able to proceed with the standard public consultations. We wish everyone and their Families good health during this time.

We again reserve all our rights.

I ██████████ and or nominees, on behalf of ██████████ wish to be heard on this submission at the Future Nillumbik Committee meeting. We will expand on this submission when we speak. If our objections and suggestions can-not be accommodated or incorporated we wish to be heard by an Independent Planning Panel appointed by the Planning Minister for such purposes.

As the Diamond Creek Major Activity Centre and Structure Plan are being reviewed, then all aspects of it should be open for public consideration. This includes full open and transparent review of the past/current mistakes, injustices that were overlooked previously.

Please also see all our previous Submissions to the Diamond Creek Urban Growth Review and study, associated Diamond Creek Township Strategy, the Diamond Creek 20/20 Structure Plan and Leisure Facilities Plan and associated processes. Also see Submission dated 24<sup>th</sup> August 2018 and tabled Submission book titled 91B that was tabled 11<sup>th</sup> December 2018 with presentation that responded to this draft at Stage One. All these interrelate with their translations into the Diamond Creek Activity Centre Zones and are therefore relevant to this New Major Activity Centre Structure Plan.

The current Diamond Creek Structure Plan 2020 is based on the erred planning foundation that dates to year 2000 when the New Format Nillumbik Planning Scheme was adopted. In this process lands in the original suburb 'urban area of Diamond Creek' and urban area of Yarrambat eg. Ironbark Rd and adjoining Pioneer Rd properties of this submission were arbitrarily back zoned in error to Rural Conservation now Environmental Rural (ERZ). They were omitted from the 'New' Diamond Creek Urban growth review (see map of areas A B C and D) and associated Diamond Creek Township strategy. Inclusion in the former process would have corrected this mistake. *This omission was the beginning of the marginalisation of Chute St that continues to this day and is only going to be further cemented and amplified by this new plan.*

Please ensure these properties (and in view like properties) are afforded equity and justice now. The outstanding zoning and mapping mistakes to the lands on Ironbark and Pioneer Rds, continuously drawn to the attention of Council and others must be corrected as a first priority prior to the proceeding of this New Structure Plan. This is to ensure there is no impact whatsoever on our land in Chute St and the acreages on Ironbark and Pioneer Rd, by Councils new Diamond Creek Activity Centre Draft Plan. In particular the erosion and redirection of urban reticulated infrastructure and capacity to provide for the new Structure Plan and Strategies that may stem from it. Failure to do so will mean the plan has a flawed and unacceptable foundation. Irrespective of any final site specific land use this mistake must be rectified equitably.

Objections to Draft Plan

**Precinct One (Chute St) 5.1 of draft is the original historic Township. Precinct One needs to be renamed as Retail Core 1, to reflect its true position at the top of the town centre hierarchy. All development policies and strategies that apply must be made to maintain this position. Any policies or strategies that have eroded this position must be returned.**

**We therefore strenuously object to both the new title as 'Chute St Commercial Strip' and its connected objective as 'retail, office, entertainment activities and as a Secondary node to the retail core in Precinct 4'. We ask for these objectives to be removed.**

The more extended retail and commercial development that occurs east of the railway (previously known as Diamond Creek East) the more detrimental impact it has on the original Precinct 1 and its retail business'.

It further disenfranchises and separates Precinct 1 from its identity as the integral town centre in its own right. It must be remembered that **Precinct 1 was there first.**

**The draft plan lists as an objective 'to ensure development retains and enhances the township's character'. Any demotion of Precinct One will surely make such objectives seem hollow and baseless.**

**The term Secondary Retail Node for Precinct One must be removed.**

Likewise, the policies and strategies need to be revised in Precinct 1 to reflect an equal primary retail, entertainment and business node.

The viability, vitality and identity of the original Diamond Creek Township (that is Precinct 1) as being part of the whole Diamond Creek Town Centre must be improved and enhanced, not destroyed and bankrupted for the benefit of the newly expanded precincts. Such impact is against all planning principles.

Precinct 1 must not become in affect a restricted business/retail area which appears to be happening already. It must be able to expand its retail and business according to free enterprise and investment decisions. All options must be properly and openly explored for the Chute st Precinct. Such changes are already happening with significant decreased rents, decreased land valuation and decreased business interests and investment in Precinct 1.

Essential township retail businesses and everything else that constitutes a township or town centre have been unfairly lobbied and moved to the new precincts eg. Post office, Pharmacy, Newsagent, Supermarket. This must be amended.

We object to the mixed use zonings that were originally applicable to Precinct 1 and 6 (previously called Precinct 5) now being extended to Precinct 3, Precinct 4 and Precinct 5! *All investment needs to be encouraged to make use of Precinct One before any incentives are offered for going elsewhere.*

It is unreasonable and unacceptable for infrastructure to be removed from Precinct 1 thereby limiting its expansion including via policies and strategies with the excuse of character of other.

We note that the original Diamond Creek Primary School has now been removed entirely from the study area. This further disenfranchises and separates Precinct 1 from its identity as the integral town centre in its own right.

We request the Activity Centre be extended to include the original township of Diamond Creek including Diamond St, Bage St and Collins St, the Primary School, the original Police Station and the original Church on Chute st. This will reinstate the original and proper identity of Diamond Creek town centre and restore balance in its expansion. This is imperative to respect the historic role of Chute St in order to comply with the stated objective of the report to maintain 'historical character'. Chute st was and is the gateway to the Activity Centre from Greensborough and Yarrambat. Diamond St is the gateway from Eltham.

